

Appendix H. Cultural Heritage Impact Report

**Glanmire to City Centre Cycle Route – Phase 2,
Cork City**

Cultural Heritage Impact Assessment

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1. Introduction

John Cronin & Associates have been commissioned by AtkinsRéalis to prepare this cultural heritage (including archaeology and architectural heritage) impact assessment report for the Glanmire to City Centre Cycle Route – Phase 2 project (hereafter ‘the proposed scheme’). The assessment is based on a programme of desktop research and site inspections of the proposed scheme.

The assessment is also based on details and mapping of the proposed scheme which were available at the time of writing in January 2024. It is noted that further details on elements of the proposed scheme will be formulated during a detailed design phase to be carried out in advance of construction and this process will include inputs by suitably qualified archaeological and architectural heritage specialists and consultations with the Cork City Council Archaeologist and Conservation Officer.

In general, the proposed scheme will extend eastwards from Penrose Quay in Cork City centre to an existing roundabout at the east end of the Lower Glanmire Road (location mapping is presented in Appendix 1 of this report). It is primarily centred on existing road carriageways and will entail the following elements:

- Segregated cycle facilities along the route.
- Footpath widening and enhancement.
- Alterations/upgrading of several existing signalised junctions/pedestrian crossings.
- Signalised junctions along the route
- New pedestrian/cycle bridge at two locations
- A pedestrian ramp at the North Ring Road
- Signage and way finding improvements along the route.
- Alterations to existing bus stops/lay-bys along the route.

Available plans and sections of the proposed scheme are also presented in drawings prepared for the application by AtkinsRéalis and these were consulted during this assessment (see footnote below for drawing reference numbers¹).

The assessment was carried out in order to assess potential impacts of the proposed scheme on the known cultural heritage resource within a study area comprising the area within the scheme boundary as well as lands extending for 50m from either side of the boundary. The review of this study area facilitated the identification of known cultural heritage constraints within the environs of the proposed scheme as well as an appraisal of the baseline cultural heritage context of the area.

The identified constraints within the study area, including archaeological sites, protected structures, buildings listed in the NIAH, have been assigned unique identifier numbers with the prefix Cultural Heritage Constraint (CHC) and these are listed in Section 4 (**Table 1**) and are mapped in Appendix 1 (Figures 11a to 11c). A single CHC number has been applied in instances where a number of individual constraints form a coherent grouping, e.g., housing terraces containing structures of similar date and form. A number of the constraints within the study area are multi-designate features which have been included in more than one record, e.g., historic country houses listed in the RMP, RPS and NIAH, and the designations and relevant reference numbers applied to each constraint are also identified in **Table 1**. Details on the identified constraints within the study area are provided in **Section 4.6**.

¹ AtkinsRéalis Drawing References: 5218242-ATK-ZZ-ZZ-SK-CE-000001 to 5218242-ATK-ZZ-ZZ-SK-CE-000009

Section 2 of the report provides details on the methodology applied during the compilation of this assessment.

Section 3 presents summary information on the legal, planning and guideline frameworks relevant to this assessment.

Section 4 presents an overview of the baseline context for the study area as well as details on cultural heritage constraints identified during desktop research and site inspections.

Section 5 presents the results of the assessment of potential impacts arising from the proposed scheme.

Section 6 presents details on recommended mitigation measures.

Appendix 1 provides extracts from historical mapping reviewed as part of the assessment and location mapping for cultural heritage constraints within the study area.

Appendix 2 provides extracts from the photographic record compiled during site inspections.

Appendix 3 provides NIAH inventory descriptions of cultural heritage constraints within the study area.

2. Assessment Methodology

2.1 Desktop Study

The assessment presents the results of a desktop review of the below published sources and datasets which was carried out in August 2023 in order to identify all recorded and potential archaeological, architectural and other cultural heritage sites/features/areas within the study area:

- Record of Monuments and Places²
- Sites and Monuments Record³
- National Monuments in State Care⁴
- Monuments with Preservation Orders⁵
- National Monuments Service Wreck Viewer⁶
- Record of Protected Structures⁷
- Architectural Conservation Areas⁸
- National Inventory of Architectural Heritage (buildings and gardens)⁹
- Archaeological Inventory of County Cork Volume 2¹⁰
- Heritage Council of Ireland Map Viewer¹¹
- Database of Irish Excavation Reports¹²
- Cartographic Sources
- Aerial/Satellite imagery¹³
- Irish National Folklore Collection¹⁴
- Placenames Database¹⁵
- UNESCO designated World Heritage Sites and Tentative List¹⁶

2.2 Site Inspection

A number of inspections of the proposed scheme were carried out in August 2023. The majority of the known constraints within the study area are located within private third-party lands and these were appraised by a combination of visual inspections from public roads, reviews of published inventory descriptions, historic mapping and aerial/satellite imagery.

² <https://archaeology.ie/publications-forms-legislation/record-of-monuments-and-places>

³ <https://maps.archaeology.ie/HistoricEnvironment/>

⁴ <https://archaeology.ie/sites/default/files/media/pdf/monuments-in-state-care-cork.pdf>

⁵ <https://archaeology.ie/sites/default/files/media/publications/po19v1-all-counties.pdf>

⁶ <https://dahg.maps.arcgis.com/apps/webappviewer/index.html?id=89e50518e5f4437abfa6284ff39fd640>

⁷ <https://www.corkcoco.ie/sites/default/files/2022-06/volume-2-heritage-amenity.pdf>

⁸ <https://www.corkcity.ie/en/cork-city-development-plan/volume-3-built-heritage-objectives/>

⁹ <https://www.buildingsofireland.ie/>

¹⁰ Power, D. (1997) Archaeological Inventory of Co. Cork. Vol. 2. South and East Cork (199)

¹¹ <https://www.heritagemaps.ie/WebApps/HeritageMaps/index.html>

¹² <https://excavations.ie/>

¹³ OSI. Google Earth and Bing Maps

¹⁴ <https://www.duchas.ie/en>

¹⁵ <https://www.logainm.ie/en/>

¹⁶ <https://www.gov.ie/en/press-release/72ef0-ministers-announce-new-world-heritage-tentative-list-for-ireland/>

2.2 Assessment of Impacts

The methodology used for this assessment is based on guidelines presented in the Environment Protection Agency (EPA, 2022) *Guidelines on the information to be contained in Environmental Impact Assessment Reports*¹⁷.

The types of effects are categorised as either being direct, indirect or no predicted impact:

- *Direct Impact* – where a cultural heritage site is physically located within the footprint of the scheme, which will result in its complete or partial removal.
- *Indirect Impact* – where a cultural heritage site or its setting is located in close proximity to the footprint of the scheme.
- *No predicted impact* – where the potential scheme will not adversely or positively affect a cultural heritage site.

The quality of an effect on the cultural heritage resource can be positive, neutral or adverse:

- *Positive Effect* – a change which improves the quality of the cultural heritage environment
- *Neutral Effect* – no change or effects that are imperceptible
- *Adverse Effect* – a change which reduces the quality of the cultural heritage resource

The values of known or potential cultural heritage constraints that may be affected by development are ranked according to the following scale: Very High; High; Medium; Low and Negligible. The National Monuments Act 1930 (as amended) does not differentiate between Record of Monuments and Places (RMPs) in terms of perceived importance and, therefore, all RMPs and, for the purpose of this assessment, all Sites and Monuments Records (SMRs) are considered to be of high value. Architectural heritage constraints which are afforded statutory protection, such as protected structures and Architectural Conservation Areas (ACAs) in accordance with the Planning and Development Act 2000 (as amended), are also considered to be of high value. Cultural heritage constraints listed solely on the NIAH do not have statutory protection, but where they comprise extant structures deemed by the NIAH to be of 'Regional' significance they have been considered to be of medium value.

The cultural heritage resource is not limited to tangible archaeological and architectural constraints and may also include other tangible and intangible receptors of cultural significance including, but not limited to, museums, art galleries, public art, heritage centres, commemorative plaques and statues, sporting facilities, religious centres, historical associations, tradition and folklore. While such constraints are not afforded protection under current legislation, unless they form a component of an existing RMP, National Monument, RPS or are included within an ACA, they all can form notable aspects of local cultural heritage significance and are considered to be of very low to medium value based on professional judgement.

The magnitude of effect on cultural heritage constraints is based on the degree of change, incorporating any mitigation measures, on the constraint and can be adverse or positive. The magnitude is ranked without regard to the value of the constraint according to the following scale: High; Medium; Low and Negligible:

- *high* applies when most or all key aspects of a constraint are totally modified.
- *medium* applies when a constraint is significantly modified.
- *low* applies when a constraint is slightly altered.

¹⁷ <https://www.epa.ie/publications/monitoring--assessment/assessment/guidelines-on-the-information-to-be-contained-in-environmental-impact-assessment.php>

- *negligible* applies to very minor changes to a constraint.

The significance of effects on cultural heritage constraints is based on an assessment of constraint value combined with the magnitude of effect and can be profound, significant, moderate, slight, not significant or imperceptible:

- *profound* applies where mitigation would be unlikely to remove adverse effects that arise where a cultural heritage site is completely and irreversibly destroyed by a proposed development.
- *significant* applies when an effect, by its magnitude, duration or intensity, alters an important aspect of the environment. It applies where part of a cultural heritage site would be permanently impacted upon, leading to a loss of character, integrity and data about the feature/site.
- *moderate* applies when a change to a cultural heritage site is proposed that, though noticeable, does not compromise the integrity of the site and which is reversible. This arises where a cultural heritage site can be incorporated into a modern-day development without damage and where all procedures used to facilitate this are reversible.
- *slight* applies to changes in the character of the environment which are not significant or profound and do not directly impact or affect a cultural heritage site.
- *not significant* effects cause noticeable changes in the character of the environment but without significant consequences.
- *imperceptible* applies where an effect is capable of measurement but does not carry noticeable consequences.

3. Legislation, Guidelines and Planning Policies

The Minister for Housing, Local Government and Heritage is presently responsible for the statutory functions and administration of the national policy in relation to archaeological heritage management. The National Monuments Service (NMS) is part of the Department and plays a key role in the protection of the archaeological heritage resource.

The management and protection of the cultural heritage resource in Ireland is achieved through a framework of international conventions and national laws and policies. This framework was established in accordance with the provisions of the 'European Convention on the Protection of the Archaeological Heritage' (the Valletta Convention) and the 'European Convention on the Protection of Architectural Heritage' (Grenada Convention) both of which were ratified by Ireland in 1997. While there is no current national legislation providing legal protection for the Irish intangible heritage resource it is noted that the UNESCO *Convention for the Safeguarding of the Intangible Cultural Heritage*, 2003, which seeks to safeguard and promote awareness of this element of cultural heritage, was ratified by Ireland in 2015.

The legislation and guidelines relevant to this assessment comprise the following:

- Historic and Archaeological Heritage and Miscellaneous Provisions Act 2023
- National Monuments Act 1930 as amended;
- Heritage Act (1995);
- National Cultural Institutions Act (1997);
- Architectural Heritage (National Inventory) and Historic Monuments (Misc) Provisions Act (1999);
- Planning and Development Act (2000) as amended;
- Department of Arts, Heritage and Gaeltacht (2011) *Architectural Heritage Protection: Guidelines for Planning Authorities*¹⁸;
- Department of Arts, Heritage, Gaeltacht and the Islands (1999) *Framework and Principles for the Protection of Archaeological Heritage*¹⁹;

The National Monuments Service (NMS) is currently based in the Department of Housing, Local Government and Heritage (DHLGH) and is responsible for the preservation, protection and promotion of Ireland's archaeological heritage. The Historic and Archaeological Heritage and Miscellaneous Provisions Act 2023 was signed into law on October 13th, 2023. The Department of Housing, Local Government and Heritage published an online guidance note²⁰ in relation to this Act in November 2023 which provides an overview of its current status, and this is summarised hereafter. While the Act is now law most of its provisions will not enter into force until the Minister of Housing, Local Government and Heritage has made one or more "Commencement Orders". This means that section 7 of the Act (which provides for the repeal of the National Monuments Acts 1930 to 2014 and related legislation) has not entered into force. Accordingly, the National Monuments Acts 1930 to 2014 and other legislation which section 7 of the Act will, when it comes into force, repeal, remain fully in force as they stood on 13th October and will continue to do so for the time being. The Act contains transitional provisions which will, if necessary, enable certain aspects of the existing National Monuments Acts 1930 to 2014 to continue in operation notwithstanding their repeal post-

¹⁸ <https://www.gov.ie/en/publication/0937a-architectural-heritage-protection-guidelines-for-planning-authorities/>

¹⁹ <https://www.archaeology.ie/sites/default/files/media/publications/framework-and-principles-for-protection-of-archaeological-heritage.pdf>

²⁰ <https://www.archaeology.ie/news/enactment-of-historic-and-archaeological-heritage-and-miscellaneous-provisions-act-2023-and>

commencement of the Act while successor provisions are being brought fully into operation. This includes provisions enabling the Record of Monuments and Places to continue to have effect pending the establishment of a new Register of Monuments.

The National Monuments Act (1930-2004), the Heritage Act 1995 and relevant provisions of the National Cultural Institutions Act 1997 therefore remain the primary means of ensuring the satisfactory protection of archaeological remains. There are a number of mechanisms under the National Monuments Acts that are applied to secure the protection of archaeological monuments. These include designating sites of national significance as National Monuments or entering them on the Register of Historic Monuments (RHM), the Record of Monuments and Places (RMP) and the Sites and Monuments Record (SMR). Preservation Orders or Temporary Preservation Orders can also be placed on endangered sites.

Section 2 of the National Monuments Act, 1930 defines a National Monument as “a monument or the remains of a monument, the preservation of which is a matter of national importance”. The State may acquire or assume guardianship of examples through agreement with landowners or under compulsory orders. Archaeological sites within the ownership of local authorities are also deemed to be National Monuments. The prior written consent of the Minister is required for any works at, or in proximity to, a National Monument or sites which are subject to a Preservation Order.

The RMP was established under Section 12 (1) of the National Monuments (Amendment) Act, 1994 and was based on the earlier SMR and RHM records. It comprises lists and maps of all known archaeological monuments and places for each county in the State and all listed archaeological sites receive statutory protection under the National Monuments Act 1994. No works can be undertaken at their locations or within their surrounding designated Zones of Notification without providing two months advance notice to the NMS.

The protection of the architectural heritage resource is provided for through a range of legal instruments that include the Heritage Act 1995, the Architectural Heritage (National Inventory) and National Monuments (Misc. Provisions) Act 1999, and the Planning and Development Act 2000. The Planning and Development Act 2000 requires all Planning Authorities to keep a ‘Record of Protected Structures’ (RPS) of special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest. Since the introduction of this legislation, planning permission is required for any works to a protected structure that would affect its character. A protected structure also includes the land and other structures within its curtilage. While the term ‘curtilage’ is not defined by legislation, the *Architectural Heritage Protection Guidelines for Local Authorities* (Department of Arts, Heritage and the Gaeltacht 2011), describes it as the parcel of land immediately associated with a structure and which is (or was) in use for the purposes of the structure. In addition, local authorities must provide for the preservation of places, groups of structures and townscapes of architectural heritage significance through designation of Architectural Conservation Areas (ACAs).

The National Inventory of Architectural Heritage (NIAH) was established as to record architectural heritage structures within the State and while inclusion in the NIAH does not provide statutory protection it is intended to advise local authorities on compilation of their Records of Protected Structures. The NIAH also includes a Survey of Historic Gardens and Landscapes which comprises a non-statutory, desk-based survey of such features.

The Cork City Development Plan 2022-2028 includes a range of objectives in relation to the protection of the archaeological resource within the city and these comprise: Objective 8.1 (Strategic Archaeology Objective), Objective 8.2 (Protection of the Archaeological Resource), Objective 8.3 (The Value of Archaeological Knowledge), Objective 8.4 (Protection of the Medieval Historic Core),

Objective 8.5 (Protection of Cork's Medieval City Wall and Defences), Objective 8.6 (Objective 8.6 (Protection of Burial Grounds), Objective 8.7 (Industrial Archaeology) Objective 8.8 (Underwater Archaeology), Objective 8.9 (Preservation of Archaeology within Open Space in Developments) and Objective 8.10 (Archaeological Management Strategy for the City). The descriptions of each of these planning objectives are available at <https://www.corkcity.ie/en/cork-city-development-plan/volume-1-written-statement/> (pages 272-274).

The Cork City Council Development Plan 2022-2028 also presents a number of objectives intended to protect the architectural heritage resource within the city and these comprise: Objective 18.17 (Conservation of the City's Built Heritage), Objective 8.18 (Reuse & Refurbishment of Historic Buildings), Objective 18.19 (Record of Protected Structures), Objective 8.20 (Historic Landscapes), Objective 8.21 (Enabling Development), Objective 8.22 (National Inventory of Architectural Heritage), Objective 8.23 (Development in Architectural Conservation Areas), Objective 8.24 (Demolition in Architectural Conservation Areas), Objective 8.25 (Recording of Structures in Architectural Conservation Areas), Objective 8.26 (Individual Buildings of Character in Suburban Areas and Villages), Objective 18.27 (Elements of Built Heritage), Objective 18.28 (Separate Access to the Upper Floors of Buildings) and Objective 8.29 (Historic Town Centre Supports). The descriptions of each of these planning objectives are available at <https://www.corkcity.ie/en/cork-city-development-plan/volume-1-written-statement/> (pages 276-280).

In addition, the Cork City Council's *North Docks Local Area Plan* (2005) contains the following overview of the Penrose and Horgan's Quay areas in the west end of the study area:

The North Docks plan area is outside the zones of archaeological importance. However, since it is largely located on reclaimed river marsh, there may be a requirement for survey and monitoring where a development will involve large-scale ground disturbance. Cork's pre-eminence as an industrial centre in the late 18th and 19th centuries has created the most tangible record of historic archaeological remains in the North Docks. Many of the quay walls, shipping offices, associated warehouses and yards still survive as do the most significant structures associated with the development of the railway from the 1840s to the present. The former ship-building and repair yards are now only to be found in the extreme eastern end of the area, and are consequently of even more significance because of the rarity of what was once an important industry in the city. The area bounded by Penrose Quay, Clontarf St., Railway St. has a distinctive character reflecting its historical development as a centre for shipping offices and warehouses (following the construction in the early 19th C of the new deeper quay along Penrose Quay). It is worth noting that despite much later rebuilding; the Penrose quay wall contains large sections of fine cut limestone, which are of significance.

4. Receiving Environment

4.1 General Overview

The western end of the proposed scheme extends into Penrose Quay which is within the modern centre of Cork city but at its nearest point the scheme is located c.830m outside the medieval walled town and is c.470m outside the east end of the Zone of Archaeological Potential surrounding the historic core of the city. This part of the city remained outside the developed urban area until the 18th and 19th centuries which saw the reclamation of riverside marshes and the subsequent development of streets, quays and dock infrastructure following improvements to the navigation of the River Lee which allowed larger ships to access the city centre rather than docking in Passage West further down the harbour. While the coverage areas of the majority of historic maps of Cork City do not extend to the study area, the detail on a number of 18th century and early 19th century maps indicate that while the city had begun to expand towards the west end of the Penrose Quay area it appears to have remained as an area of estuarine marsh until the early decades of the 19th century (e.g. O' Connor 1774, Beauford 1801 and Holt 1832) (Figures 1-3). These maps also indicate that the road to Glanmire was in place by at least the 18th century and the detail on the Holt map of 1832 indicates that slipways associated with shipbuilding activity had been developed in the area now occupied by McMahon Builders Provisions on Water Street (see **Section 4.6**)

The study area also contains a section of the former Cork, Youghal & Queenstown railway line which remains in use as an active transport infrastructure. The construction of the railway line commenced in 1854 in order to connect Cork and Youghal, with a branch line to Queenstown, and the section within the study area was constructed along the riverbank on the south side of the Lower Glanmire Road. The line to Youghal opened to passengers in 1861 while the Queenstown (now Cobh) branch opened during the following year. The railway operation immediately encountered financial issues and was sold to the Great Southern and Western Railway Company in 1865. While the section of the line extending to Youghal closed to passenger traffic in 1963, the line to Cobh, including the section within the study area, has continued as a commuter service and the section to Midleton was reopened in recent years. The proposed scheme will not require any interventions to this active railway line. While the west end of the study area also extends into the Kent Railway Station property, the proposed scheme does not enter this property and will not require any interventions to its random rubble boundary wall which extends along a modern footpath on the north side of the road.

The Tivoli docks and industrial park in the eastern part of the study area are situated in an area of reclaimed riverine slob lands which had been subject to extensive dredging works in the 19th century. The reclamation of the area commenced in the early 20th century when the Cork Harbour Commissioners purchased the slob lands between Tivoli and Dunkettle for the deposit of dredged material. The docks were then established in the reclaimed area by the Port of Cork Company in the mid-20th century. A review of this section of the river on the first edition 6-inch map (published 1845) and 25-inch OS map (published 1902), which pre-date the reclamation works, revealed no constructed features such as quays, wharfs or piers and the location is shown as vacant tidal slob lands to the north of the main channel (Figure 4c and Figure 5c). The second edition 6-inch OS map (published 1956) shows the early stages of the reclamation process with a breakwater shown within the river at the west end of the area which also contained an oil storage facility in reclaimed ground on the north side of the breakwater (Figure 6c). The area to the east of the oil store is still labelled as 'Covered by Spring Tides' indicating that reclamation works had not been completed at the time of survey works.

4.2 National Monuments in State Care

There are no National Monuments in State Care located within the study area.

4.3 Recorded Archaeological Sites

There are no recorded prehistoric, early medieval or late medieval archaeological sites located within the study area and the proposed scheme is located c.470m outside the east end of the Zone of Archaeological Potential surrounding the historic core of Cork City as defined by the National Monuments Service. There are two recorded archaeological sites located within the study area and these comprise a country house (CHC 10) and a gateway (CHC 19), both of which date to the early 19th century. Both of structures are listed as protected structures and are also included in the NIAH (see **Section 4.6** and **Table 1**). In addition, a review of the National Monument Service Wreck Viewer revealed that it does not contain any entries for the presence of recorded shipwrecks within the section of the River Lee located within the study area.

4.4 Database of Excavation Reports

A review of the Database of Excavations Reports revealed that no licensed archaeological investigations have been carried out within any of the lands within the Study Area. The Database contains two entries describing programmes of archaeological monitoring of pipeline trenching within the River Lee at Tivoli as part of the Cork Main Drainage Scheme and nothing of archaeological significance was uncovered during either investigation^{21 22}. The Database also contains a description of the results of an underwater survey of potential in-channel works to facilitate the construction of a proposed bridge spanning the River Lee near the west end of the Tivoli docks area²³. The survey of this section of the channel revealed nothing of archaeological significance. Recent archaeological excavations carried out as part of the N25 Dunkettle Interchange Motorway Improvement Scheme which is located outside the east end of the study area revealed a burnt mound site with associated features and indicates late prehistoric activity occurring within lands adjacent to the riverside²⁴.

4.5 National Museum Topographical Files

A review of the topographical file archive held at the National Museum's premises in Kildare Street, Dublin was carried out for the townlands within the study area in July 2023. This revealed that the archive does not contain any files recording the discovery of archaeological objects within any of these townlands.

4.6 Architectural Heritage

The following summary of the recorded architectural heritage constraints within the study area is intended as a high-level overview and the descriptions extend from the west end of the study area to the east end. The summaries include information sourced from NIAH inventory entries, published sources, reviews of the first edition 6-inch (1845), 25-inch (1902) and Cassini edition (1956) Ordnance Survey (OS) maps and site inspections.

The proposed scheme is not located within, or immediately adjacent to, an Architectural Conservation Area. There are a number of protected structures located within the study area, including examples also listed in the NIAH, which also lists a number of structures that have not been included in the Record of Protected Structures (**Table 1**). The NIAH has assigned a Regional rating to all of the listed structures located within the study area.

Two of the identified architectural constraints identified within the study area extend within the scheme boundary and these comprise Horgan's quay/wharf (CHC 3) and a slipway (Appendix – 1 CHC

²¹ <http://excavations.ie/report/2002/Cork/0007602/>

²² <http://excavations.ie/report/2001/Cork/0006106/>

²³ <https://excavations.ie/report/2008/Cork/0019229/>

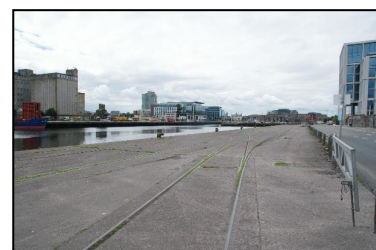
²⁴ <http://excavations.ie/report/2018/Cork/0027264/>

6) within the McMahons Builders Providers property located to the east of Horgan's Quay (**Table 1** and **Figure 11a**).

The majority of the recorded architectural heritage constraints within the west end of the study area comprise 19th century buildings and structures associated with 19th century transport and dock infrastructure within the environs of Penrose and Horgan's Quays. The section of the study area extending eastwards along Lower Glanmire Road as it leaves the city centre contains buildings and gardens associated with 18th and 19th century country house properties in elevated settings along a ridgeline on the north side of the of the road. These are variously listed in the SMR/RMP and the Record of Protected Structures as well as in the NIAH (**Table 1**). A number of the NIAH-listed structures and gardens within the area also form part of the curtilages of protected structures, including gate lodges, boundary features and demesne lands. The listed buildings within the area typically comprised the homes of wealthy city merchants and professionals who developed country house residences in what would have been part of the agricultural outskirts of the increasingly overcrowded city centre during the 18th and 19th century. The proposed scheme does not extend within any of the grounds associated with the listed country houses to the north of Lower Glanmire Road and is contained within the road carriageway as well as within an existing road within the Tivoli docks area, which comprises an area of land reclaimed from the River Lee during the 20th century.

Work commenced on the construction of a new quay wall along Penrose Quay middle of the 19th century it and was further extended in 1894 (Rynne 1999). The existing Penrose quay wall was extensively reconstructed in the late 20th century and is not listed in the RPS or NIAH but, as noted in the North Dock Local Area Plan of 2005, this modern structure contains sections of fine cut limestone, which are of significance. The modern quay façade seals the original quay wall which comprises an outer facade of cut stone with bonded rubble masonry to the rear. This original wall retained is approx. 1m below modern street level at a distance of 3m behind the new quay wall (Ove Arup 1980). The area along the north side of the road along Penrose Quay has been extensively redeveloped with commercial buildings in recent decades but still contains the former Cork Steam Packet Office building (CHC 1) which is a seven-bay, two-storey limestone ashlar building constructed in c. 1833. A railway station warehouse (CHC 2) of cut limestone construction is also located on the north side of the road along Penrose Quay and this was built in c.1855.

The limestone quay wall and associated wharf (Appendix 1 - CHC 3) along Horgan's Quay were constructed in the 19th century with later extensions of concrete construction. These quay features are not protected structures but are listed in the NIAH (see **Table 1**). The concreted surfaces of the wharves contain crane rails, metal mooring posts and a large, corrugated warehouse at the east end, which is only depicted on the 1956 OS map indicating it dates to the first half of the 20th century. While the proposed scheme extends along the roadside section of the concreted surface of the wharf, this will not entail any interventions to the timber wharf structure, quay wall, mooring posts or warehouse by the cycleway or any associated features such as pedestrian guardrails which will set back from the quay walls. A section of crane rails (along the wharf does extends into the scheme boundary and these will be retained *in situ* with their future treatment to be formulated as part of the detailed design process in conjunction with the Cork City Council Archaeologist and Conservation Officer.



The only location where the proposed scheme extends off existing roadways comprises a section that extends through the McMahons Builders Providers property at the east end of Horgan's Quay which fronts onto Water Street at west and Lower Glanmire Street at north. This property was the site of a number of shipbuilding operations during the 19th century, and still retains a number of structures

associated with this activity which are listed as protected structures or are included within the NIAH. These comprise a Former Harbour Master's House (Appendix 1 - CHC 5), a Former Port of Cork workshop (CHC 7) and a slipway (CHC 6) (**Table 1** and **Figure 11a**). As noted by Rynne (1999), the property during middle of the 19th century contained shipbuilding and repair operations operated variously by Joseph Wheeler, Stephen Hickson and George Robinson and a number of slipways and buildings are depicted within the area on the 1845 OS map (Figure 8). Leland (2001, 34) notes that the George Robinson and Co. Waterside Dock was specially favoured by English orders for steam and sail ships during the 1840a while by the 1850s the operation had also begun the construction of steel ships. Robinson appears to have been the sole owner of the operations in the yard by 1863 but by 1869 had ceased construction works and the company ceased trading in 1877 (*ibid.*, 36). The Cork Harbour Commissioners completed the transfer of the property to their own control in 1877 and converted Robinson's operation into a repair yard which continued in operation into the 20th century. They also constructed the extant workshops (CHC 7) and Former Harbour Master's House (CHC 5) along the Lower Glanmire Road frontage in the north end of the yard. The detail on the 1902 OS map indicates that the slipways within the yard were still present at the start of the 20th century and it also shows a number of buildings within the property which included a now demolished sawmill in the southwest corner of the property in an area now occupied by a car park (Figure 8). The 1902 map also shows the presence of a Cork Steam Pkt. Co.'s Dock Yard in the west end of the property in the area now occupied by the modern McMahon's retail store and its adjacent carpark. The detail on the 1956 OS map shows that most of the slipways had been infilled by the middle of the 20th century with only the existing open slipway (CHC 6) to the east of the modern builders' provision retail premises still depicted (Figure 8). Rynne (1999) notes that this appears to have been associated with Joseph's Wheeler's operation in the yard during the mid-19th century and that it has been subject to extensive repairs. The 1956 map also contains a 'Wharves' annotation along the riverside section of the property which may have been added after the slipways were infilled and these remain along the river's edge but are in derelict condition with sections in various stages of collapse. An inspection of the Former Harbour Master's House (CHC 5), Former Port of Cork workshops (CHC 7) and the slipway (CHC 6) from the open concrete yard area within the property revealed that each of these structures remains extant.



The Former Port of Cork workshops (Appendix 1 - CHC 7) remains in active use as a builder's provision storage building located outside the scheme boundary (Plate 21). An inspection of the building from the yard area to the south revealed that it comprises a late 19th century, twelve-bay, single-storey double-height structure of random sandstone construction with dressed limestone quoins. The Former Harbour Master's House (CHC 5) located to the west of the workshop also remains extant but appears to have been boarded up and it was obscured from the yard to the south by a large stack of timber pallets during the site inspection (Plate 20). This building is located 50m outside the scheme boundary.

The stone-built slipway (Appendix 1 - CHC 6) also remains extant within a fenced off area that was not accessible at the time of inspection. It is orientated northwest to southeast (c.70m long by 12m wide) and is open to the river to the south. A visual inspection of the structure from outside the fencing revealed that it appears to remain in good condition and Rynne (1999) has noted that the structure appears to have subject to repair (Plates 16, 17 and 18). There are no evident traces of collapse within the visible sections of the structure. While much of the stonework along the sides of the slipway as well as its sloping



base, which rises slightly to the landward side, were obscured by extensive seaweed cover at a time of inspection during low tide, where exposed the sidewalls are of random rubble sandstone construction with sections of concrete repair visible in upper sections. While the base of the slipway was also obscured by seaweed at the time of inspection the remains of two parallel metal slideways were visible along the length of the slipway base. The ground surfaces within the sections of the yard adjacent to the slipway are covered by modern concrete slabs, which are partially obscured by stockpiled stored materials, with no surface traces of associated structures noted.

The proposed scheme will not require any direct interventions to any of the designated architectural heritage constraints within the McMahon property or to the remaining wharf features along the river front. The scheme will include the construction of a clear span cycle bridge over the south end of the extant slipway (Appendix 1 - CHC 6) and this will stand on footings set back from both sides of the slipway structure in order to avoid any impacts on its stonework. While the detailed design of the foundations for the bridge has not been formulated at the time of writing, the design team has confirmed that they will likely be formed by piles with some localised excavation for pile caps which will be set at a minimum distance of 2m from the edges of the slipway. The bridge will be designed so that will be lifted in place on the footings by crane and so that it can be periodically lifted to facilitate any proposed future use of the slipway as an amenity feature.

The creation of the cycleway within the yard area will require excavation works to remove the existing modern concrete slabs on the footprint of the cycleway and will also require the removal of extant sections of random rubble boundary walling delimiting the south end of the Water Street frontage and a section on Lower Glanmire Road to the east of the workshop (Appendix 1 - CHC 7).

An inspection of the route of the proposed scheme within other areas of the concrete surfaced yard within the McMahon's property did not reveal any extant features of cultural heritage interest, including at the locations of demolished 19th century buildings and the other slipways shown on historic OS maps. However, areas of the yard surface were covered by stockpiled stored materials and also contain areas of thick weed overgrowth (Appendix 2 - Plates 14 and 15). The potential exists that these may obscure features of cultural heritage interest on the obscured areas of the yard surface. The potential also exists for the presence of sub-surface remains of structures associated with the 19th century shipyard beneath the modern concrete yard, including slipways and structures visible on historic OS maps (see Figure 8 in Appendix 1).

The proposed scheme will require the removal of a c.15 to 20m long section of a c.3m high boundary wall along the south end of the west side of McMahon property that fronts onto Water Street (Appendix 2 - Plates 9, 10, 11, 12 and 13). The wall is of random rubble sandstone construction with occasional inclusions of cut limestone. This section comprises the only fully extant section of the western boundary wall in this area and the remainder of the section to the north has been reduced and now only retains the basal (c.0.8m high) remains that is now topped by a modern fence line. A review of Google Street Views of this area revealed that this extensive reduction of the boundary wall occurred at some point between 2011-2014 (Appendix 1 - Figures 10a and 10b). The north and south terminals of the fully extant section of the wall are formed brickwork with the example at the north end a modern inclusion which was likely added to create a defined area on the truncated line of the wall. The east-facing (internal) façade of the wall contains traces of a now demolished concrete attached structure and no other traces of features or blocked openings are visible on this section of the wall. A metal quay post and limestone river steps are located on the section of Horgan's Quay to the south of the section of the boundary wall to be removed and these are located outside the scheme boundary.



The section of the random rubble sandstone boundary wall along the east end of the north side of the McMahon's yard to be removed as part of the proposed scheme measures c.11m in length by c.3m high (Appendix 2 - Plates 22, 23, 24 and 25). The wall extends to the northeast corner of the Former Port of Cork workshops (CHC 7) but does not appear to have been directly bonded to that building (Plate 26). This section of the boundary wall follows a slightly curvilinear line which matches the line of the boundary wall shown in this area on the 1902 OS map but does not appear to be present on the 1845 map, indicating that the wall may have been constructed at the same time as the adjacent workshop in 1890.



The section of the study area around the proposed scheme as it then extends eastwards along Lower Glanmire Road, and along an existing access road within the 20th century Tivoli Docks area, contains a number of 19th century housing terraces which are listed as protected structures and/or included in the NIAH. These are all located on the north side of the road and comprise Myrtle Hill, Bellvue, Woodville Villas and Tivoli Gardens (Appendix 1 - CHC 9, CHC 12, CHC 14 and CHC 17) (see **Table 1**). All of these housing terraces are located outside the boundary of the proposed scheme and no interventions to any examples are proposed. This section of the study area also contains two detached 19th century residences, named Carrig House (Appendix 1 - CHC 10) which is a recorded archaeological site and a protected structure, and Belvedere Lodge, which is listed in the NIAH. The proposed scheme will require no interventions to these structures and does not enter their properties. The study area also contains three listed gateway structures, which are associated with country houses located outside the study area, and these comprise Lotabeg gate lodge (CHC 19), Lotamore gate lodge (Appendix 1 - CHC 20) and Lota Park gate lodge (Appendix 1 - CHC 21). These gateway structures are all located on the north side of the Lower Glanmire Road and the proposed scheme will not result in interventions to any examples and it also does not extend within their associated properties.

The section of the study area extending along Lower Glanmire Road also contains three railway footbridges (Appendix 1 - CHC 11, CHC 13 and CHC 15) and a stepped laneway (CHC 8) which are listed in the NIAH. The study area also contains a freestanding cast-iron post box dating to c.1890 (CHC 16) which is located on Trafalgar Hill to the north of Lower Glanmire Road. While the proposed scheme extends close to these constraints, all are located outside the scheme boundary and no interventions to any examples are proposed.

Table 1: Cultural heritage constraints located within study area

CHC	RMP	RPS	NIAH	Class	Distance from scheme boundary
CHC 1	n/a	PS281	20506339	Steam Packet Office	Adjacent to north
CHC 2	n/a	PS929	20506289	Warehouse	30m to north
CHC 3	n/a	n/a	20506358	Quay/Wharf	Within
CHC 4	n/a	PS926	20507180	Locomotive shed	50m to north
CHC 5	n/a	PS922	n/a	Former Harbour Master's House	50m to north
CHC 6	n/a	PS923	n/a	Patent Slipway	Within
CHC 7	n/a	PS933	20507163	Former Port of Cork workshops	Adjacent to west
CHC 8	n/a	n/a	20507128	Steps	Adjacent to north
CHC 9	n/a	PS726 to 732	20863180 & 20863148 to 20863153	Myrtle Hill Terrace	25m to north

CHC	RMP	RPS	NIAH	Class	Distance from scheme boundary
CHC 10	CO074-101----	PS624	20863157	Carrig House	25m to north
CHC 11	n/a	PS741	20863156	Foot bridge	Adjacent to north
CHC 12	n/a	PS625	20863158	Bellevue Villas Housing Terrace	20m to north
CHC 13	n/a	PS473	20863156	Foot bridge	Adjacent to north
CHC 14	n/a	PS734 to 739	20863163	Woodhill Villas Housing Terrace	20m to north
CHC 15	n/a	PS742	20863164	Foot bridge	Adjacent to north
CHC 16	n/a	n/a	20864008	Post box	Adjacent to north
CHC 17	n/a	n/a	20864029	Tivoli Gardens Housing Terrace	10m to north
CHC 18	n/a	n/a	20864009	Belvedere Lodge	20m to north
CHC 19	CO074-025----	PS626	20864012 20864013	Lotabeg gate and lodge	50m to north
CHC 20	n/a	PS1241	20864019	Lotamore gate lodge	50m to north
CHC 21	n/a	PS1187	20864022	Lota Park gate lodge	50m to north

5. Impact Assessment

The potential impacts on identified cultural heritage constraints within the study area are identified in **Table 2** below. In summary, there are two recorded archaeological sites located within the study area and these comprise 19th century buildings which are also listed as protected structures (CHC 10 & CHC 19). Neither of these archaeological sites are located within or immediately adjacent to the scheme boundary. The proposed scheme at its nearest point is located c.470m outside the Zone of Archaeological Potential surrounding the historic core of Cork City in areas that were subject to reclamation works during the 19th and 20th centuries. It is therefore concluded that proposed scheme will result in a neutral effect on the known archaeological resources. The potential exists for the presence of sub-surface remains of features of archaeological and/or built heritage significance within the scheme boundary, particularly within the McMahon Builder's Provision property, that may be subject to potential negative effects resulting from ground works at their locations. While the existence, nature and extent of any such sub-surface features is unknown the potential for negative effects will require mitigation.

The majority of the proposed scheme extends within modern road carriageways, the construction of which has likely impacted on sub-surface stratigraphy, and the construction phase of the scheme will not require extensive ground works that will have the potential to impact on any unrecorded, sub-surface archaeological remains within modern road carriageways. The proposed scheme will also not require any works within any watercourses or inter-tidal area and no construction phase impacts on potential unrecorded underwater archaeological remains are, therefore, predicted.

The proposed construction methodology within the McMahon Builder Provision yard will entail the removal of two sections of boundary wall and sections of a modern concrete slab yard surface within the footprint of the cycleway and the footings for a bridge over the open slipway in this area (CHC 6). While these interventions will result in a direct, adverse, permanent, slight effect on the undesignated boundary wall, the proposed scheme will also result in a slight-moderate positive effect by facilitating public access to this historic property, including allowing views of the remaining 19th century structures within the yard area that are located outside the scheme boundary. While additional information on the proposed construction and design of the bridge over the slipway (CHC 6) will be formulated during the detailed design phase, no direct interventions to the structure will be undertaken and the bridge foundations will be set at a minimum of 2m from its edges. The potential exists that the proposed scheme will result in a slight to moderate adverse effect on the setting of this structure and this will require design inputs by the appointed built heritage specialist in consultation with the Cork City Council Archaeologist and Conservation Officer. No interventions to quayside structures, including walls or timber wharves, along the riverside area within the yard will be carried out as part of the proposed scheme.

A review of historical OS maps of the McMahon Builder Provision premises revealed that they depict a number of other structures, including slipways and now demolished buildings, associated with 19th shipbuilding activities within the property that no longer retain any surface remains (Figure 8). The potential exists that ground works along the route of the cycleway during the construction phase of the proposed scheme will expose sub-surface remains of any surviving traces of such features located beneath the modern yard surface and this will require mitigation (see **Section 6**).

The proposed scheme does not extend within, or immediately adjacent to, any Architectural Conservation Areas. There is a range of structures listed as Protected Structures and/or included in the NIAH located within the study area and these a number of examples located adjacent to, but outside, the scheme boundary. No direct impacts on any of these constraints are predicted (see **Table 2**). Two

designated cultural heritage constraints extend within the scheme boundary and these comprise a wharf on Horgan's Quay listed in the NIAH (CHC 3) and a 19th century slipway (CHC 6), listed as a Protected Structure, within the McMahon Builder Provision premises on Water Street and potential impacts on these constraints are identified in **Table 2**.

The proposed scheme will also include the creation of sections of pedestrian guardrails along riverside areas of the cycleway on Horgan's Quay, which will be set back from the existing quay walls. This will have the potential to result in adverse visual effects on the setting of the quayside. Detailed designs of the morphology, exact locations and foundation details for these proposed safety features will be formulated during detailed design and an accurate assessment of the significance of any potent adverse effects is, therefore, not currently considered feasible but may potentially range from slight to moderate in range. It is noted that the proposed pedestrian guardrails are intended as interim safety measures that will be removable in order to facilitate any future North Dock public realm design proposals, and potential negative visual effects on the setting of the quayside arising from this element of the proposed scheme will, therefore, be reversible.

Table 2: Assessment of potential impacts on cultural heritage constraints

CHC	Class/Name	Designation	Distance from scheme	Value	Proposed Intervention	Type of Impact	Quality of impact	Magnitude of Impact	Duration of Impact	Significance of Impact
CHC 1	Steam Packet Office	RPS 281 NIAH 20506339	20m	High	None	Neutral	n/a	n/a	n/a	None
CHC 2	Warehouse	RPS 929 20506289	30m	High	None	Neutral	n/a	n/a	n/a	None
CHC 3	Quay/Wharf	NIAH 20506358	Within	Medium	Creation of cycleway. Potential interventions to crane rails. Localised pedestrian guardrails	Direct	Adverse	Potential Low	Permanent (reversible)	Potential slight to moderate
CHC 4	Locomotive shed	RPS 926 NIAH 20507180	70m	High	None	Neutral	n/a	n/a	n/a	None
CHC 5	Harbour Master's House	RPS 922	70m	High	None	Neutral	n/a	n/a	n/a	None
CHC 6	Slipway	RPS 923	Adjacent	High	Removable bridge with footings set at a minimum of 2m back from slipway on both sides	Indirect	Adverse	Low	Permanent (reversible)	Slight
CHC 7	Former Port of Cork workshops	RPS 923 NIAH 20507163	Adjacent	High	Removal of section of adjacent boundary wall	Indirect	Adverse	Slight	Permanent	Slight
CHC 8	Steps	NIAH 20507128	Adjacent	Medium	None	Neutral	n/a	n/a	n/a	None
CHC 9	Housing Terrace	RPS 726 to 732 NIAH 20863180 and 20863148 to 20863153	35m	High	None	Neutral	n/a	n/a	n/a	None
CHC 10	Carrig House	CO074-101----	30m	High	None	Neutral	n/a	n/a	n/a	None

CHC	Class/Name	Designation	Distance from scheme	Value	Proposed Intervention	Type of Impact	Quality of impact	Magnitude of Impact	Duration of Impact	Significance of Impact
		RPS 624 NIAH 20863157								
CHC 11	Foot bridge	RPS 741 NIAH 20863156	Adjacent	High	None	Neutral	n/a	n/a	n/a	None
CHC 12	Housing Terrace	RPS 625 NIAH 20863158	20m	High	None	Neutral	n/a	n/a	n/a	None
CHC 13	Foot bridge	RPS 473 NIAH 20863156	Adjacent	High	None	Neutral	n/a	n/a	n/a	None
CHC 14	Housing Terrace	RPS 734 to 739 NIAH 20863163	25m	High	None	Neutral	n/a	n/a	n/a	None
CHC 15	Foot bridge	RPS 742 NIAH 20863164	6m	High	None	Neutral	n/a	n/a	n/a	None
CHC 16	Post box	NIAH 20864008	60m	Medium	None	Neutral	n/a	n/a	n/a	None
CHC 17	Housing Terrace	NIAH 20864029	70m	Medium	None	Neutral	n/a	n/a	n/a	None
CHC 18	Belvedere Lodge	NIAH 20864009	75m	Medium	None	Neutral	n/a	n/a	n/a	None
CHC 19	Lotabeg gate and lodge	CO074-025---- RPS 626 NIAH 20864012 & 20864013	50m	High	None	Neutral	n/a	n/a	n/a	None
CHC 20	Lotamore gate lodge	PS1241 NIAH 20864019	50m	High	None	Neutral	n/a	n/a	n/a	None
CHC 21	Lota Park gate lodge	PS1187 NIAH 20864022	50m	High	None	Neutral	n/a	n/a	n/a	None

6. Recommended Mitigation Measures

Please Note: The mitigation measures presented in this section are recommendations based on available information on the proposed scheme at the time of writing in January 2024 and are subject to the approval of the National Monuments Service and the Planning Authority.

General Mitigation Strategies

It is noted that the detailed design of elements of the proposed scheme will be formulated in advance of the construction phase and it is, therefore, recommended that a suitably qualified archaeologist and built heritage specialist should be appointed to provide specialist inputs to the detailed design process and to liaise with the Cork City Council Archaeologist and Conservation Officer in relation to scheme proposals.

The construction phase works will include the creation of a cycleway along Horgan's Quay (CHC 3) including sections close to the quay wall and along extant wharves that contain crane rails and mooring posts. Works will also result in the removal of two sections of the random rubble boundary wall around the McMahon Builder Provision premises (formerly a 19th century shipyard) and the installation of a clear span bridge over an extant slipway (CHC 6) within that property. The nature and extent of proposed ground works and other interventions within Horgan's Quay and the McMahon property will be formulated during the detailed design phase to be carried out following grant of planning and in advance of the construction phase of the proposed scheme. As noted above, it is recommended that the appointed archaeological and built heritage specialists should review and advise on the formulation of detailed construction design proposals in these areas, including the methodology and extent of any proposed interventions within the environs of quayside features, and that this be carried out in consultation with the Cork City Council Archaeologist and Conservation Officer. It is also recommended that the appointed specialists should supervise and record construction phase works in these areas. They should also prepare content for inclusion in site inductions that will identify the locations of cultural heritage constraints within the environs of work areas and clearly detail construction phase restrictions relevant to these constraints (e.g. avoidance of constraints as storage areas and traffic routes as well as the requirement for attendance of specialist heritage supervision during works within their environs).

It is also recommended that the appointed built heritage specialist should also carry out a detailed written and photographic record of the existing streetscape along Horgan's Quay and within the McMahon Builder Provision premises in advance of the commencement of the construction phase, and this should include a written, drawn and photographic record of the sections of the McMahon yard boundary wall that will be removed. An annotated photographic record of these areas should also be compiled at the conclusion of the construction phase in order to create a thorough record of the amendments to the existing environment within these areas. While no direct interventions to the Former Harbour Master's House (CHC 5), Slipway (CHC 6) and Former Port of Cork Workshops (CHC 7) within the McMahon Builder Provision premises are proposed as part of the scheme, it is recommended that clearly signed, fenced off cordons be established around these structures as well as around any other cultural heritage features within the environs of constructions areas (such as quayside features), for the duration of the construction phase as a protective measure. All information compiled during built heritage inputs to the proposed scheme should be compiled in a report to be submitted to Cork City Council at the conclusion of the construction phase which should include written, drawn and photographic records.

The proposed scheme will also require the removal of sections of existing modern ground surfaces within the footprint of the cycleway in the McMahon Builder Provision premises and the potential exists that sub-surface remains of features or structures associated with 19th century quayside and shipbuilding activities may be located beneath modern ground surface within this area, including buried slipway remains, rail features, cobbled surfaces and other structural remains. It is, therefore, recommended that all ground works, including any geotechnical site investigations, within these areas should be monitored by a suitably qualified archaeologist under licence by the National Monuments Service (NMS). This should include an inspection of the existing ground surface within the yard following clearance of existing overgrowth and in advance of any ground excavation works.

In the event that any features of archaeological or built heritage interest are exposed during archaeological monitoring, ground works should be halted at the location and the features should be cordoned off and recorded by the archaeologist who will then consult with the Cork City Council Archaeologist and the NMS to determine appropriate further mitigation measures, such as preservation in situ (by avoidance) or preservation in record (by excavation). All information compiled during the archaeological inputs during construction phase should be collated in a report to be submitted to Cork City Council and the NMS which should include written, drawn and photographic records.

Constraint Specific Mitigation Measures

Table 3 presents a list of recommended mitigation measures for the locations of the identified cultural heritage constraints within the study area.

Table 3: Recommended mitigation measures for cultural heritage constraints located within study area

CHC	Designation	Class/Name	Distance from scheme boundary	Mitigation Measure
CHC 1	RPS 281 NIAH 20506339	Steam Packet Office	Adjacent to north	Location to be identified during site inductions
CHC 2	RPS 929 20506289	Warehouse	30m to north	Location to be identified during site inductions
CHC 3	NIAH 20506358	Quay/Wharf	Within	Inputs to detailed design by a built heritage specialist and consultations with Cork City Council Archaeologist and Conservation Officer in relation to detailed proposals. Compilation of advance and post-construction built heritage record by the appointed specialist. Location to be identified during site inductions. Supervision of construction phase by built heritage specialist, including establishment of protective cordons around any historic quayside features within environs of work areas.
CHC 4	RPS 926 NIAH 20507180	Locomotive shed	50m to north	None required

CHC	Designation	Class/Name	Distance from scheme boundary	Mitigation Measure
CHC 5	RPS 922	Former Harbour Master's House	50m to north	Establishment of protective cordon around structure in advance of any traffic movement within environs of building, if required.
CHC 6	RPS 923	Slipway	Within	<p>Inputs to detailed design of bridge (including foundation works) by built heritage specialist and consultations with Cork City Council Archaeologist and Conservation Officer in relation to detailed proposals.</p> <p>Location to be identified during site inductions</p> <p>Establishment of protective cordon around slipway for duration of works.</p> <p>Compilation of advance and post-construction built heritage record by the appointed specialist.</p> <p>Archaeological monitoring of ground works for bridge foundations and in surrounding yard area</p>
CHC 7	RPS 923 NIAH 20507163	Former Port of Cork workshops	Adjacent to west	<p>Location to be identified during site inductions</p> <p>Removal of section of adjacent yard boundary wall to be supervised/recorded by built heritage specialist.</p> <p>Establishment of protective cordon around section of building adjacent to works.</p> <p>Archaeological monitoring in surrounding yard area</p>
CHC 8	NIAH 20507128	Steps	Adjacent to north	Location to be identified during site inductions
CHC 9	RPS 726 to 732 NIAH 20863180 and 20863148 to 20863153	Myrtle Hill Terrace	25m to north	Location to be identified during site inductions
CHC 10	CO074-101---- RPS 624 NIAH 20863157	Carrig House	25m to north	Location to be identified during site inductions
CHC 11	RPS 741 NIAH 20863156	Foot bridge	Adjacent to north	Location to be identified during site inductions
CHC 12	RPS 625 NIAH 20863158	Bellevue Villas Housing Terrace	20m to north	Location to be identified during site inductions

CHC	Designation	Class/Name	Distance from scheme boundary	Mitigation Measure
CHC 13	RPS 473 NIAH 20863156	Foot bridge	Adjacent to north	Location to be identified during site inductions
CHC 14	RPS 734 to 739 NIAH 20863163	Woodhill Villas Housing Terrace	20m to north	Location to be identified during site inductions
CHC 15	RPS 742 NIAH 20863164	Foot bridge	Adjacent to north	Location to be identified during site inductions
CHC 16	NIAH 20864008	Post box	Adjacent to north	Location to be identified during site inductions
CHC 17	NIAH 20864029	Tivoli Gardens Housing Terrace	10m to north	Location to be identified during site inductions
CHC 18	NIAH 20864009	Belvedere Lodge	20m to north	Location to be identified during site inductions
CHC 19	CO074-025---- RPS 626 NIAH 20864012 & 20864013	Lotabeg gate and lodge	50m to north	Location to be identified during site inductions
CHC 20	PS1241 NIAH 20864019	Lotamore gate lodge	50m to north	Location to be identified during site inductions
CHC 21	PS1187 NIAH 20864022	Lota Park gate lodge	50m to north	Location to be identified during site inductions

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<http://www.corkpastandpresent.ie/>

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Appendix 1 Figures

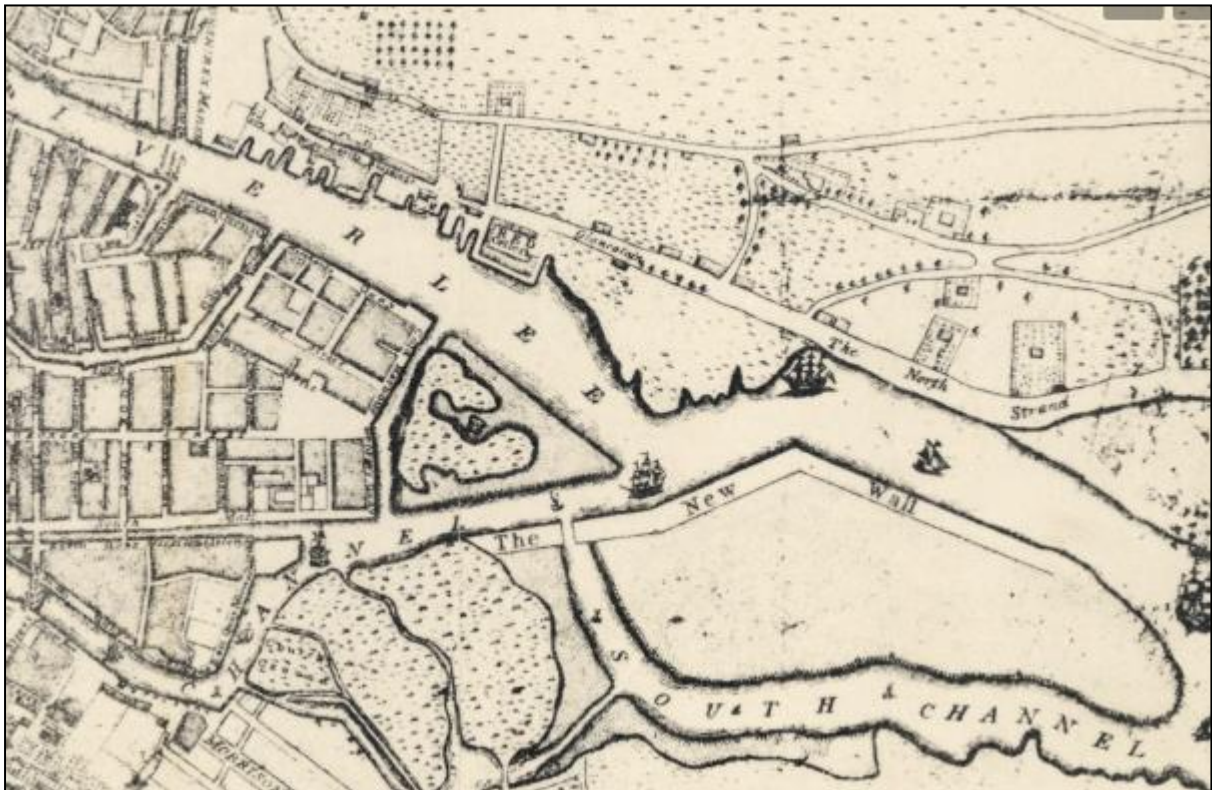


Figure 1: O'Connor's map of 1774 showing west end of study area as riverside marsh to south of road to Glanmire

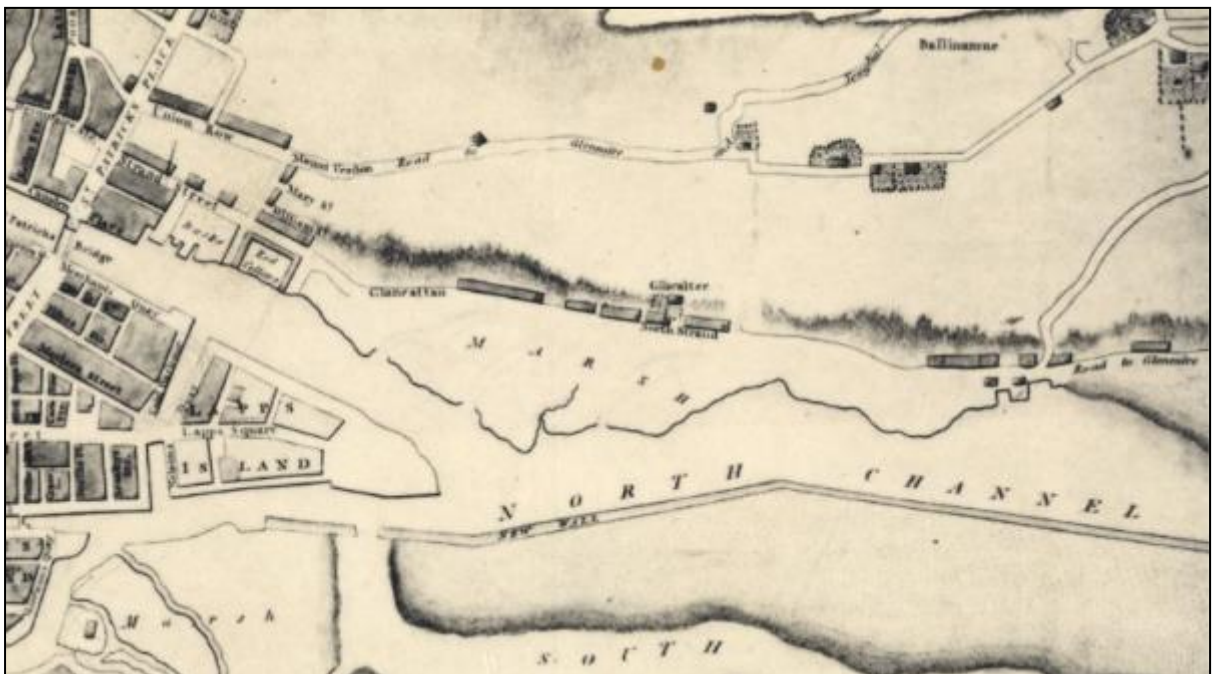


Figure 2: Beauford's map of 1801 showing the Penrose/Horgan Quay areas remaining as areas of riverside marsh

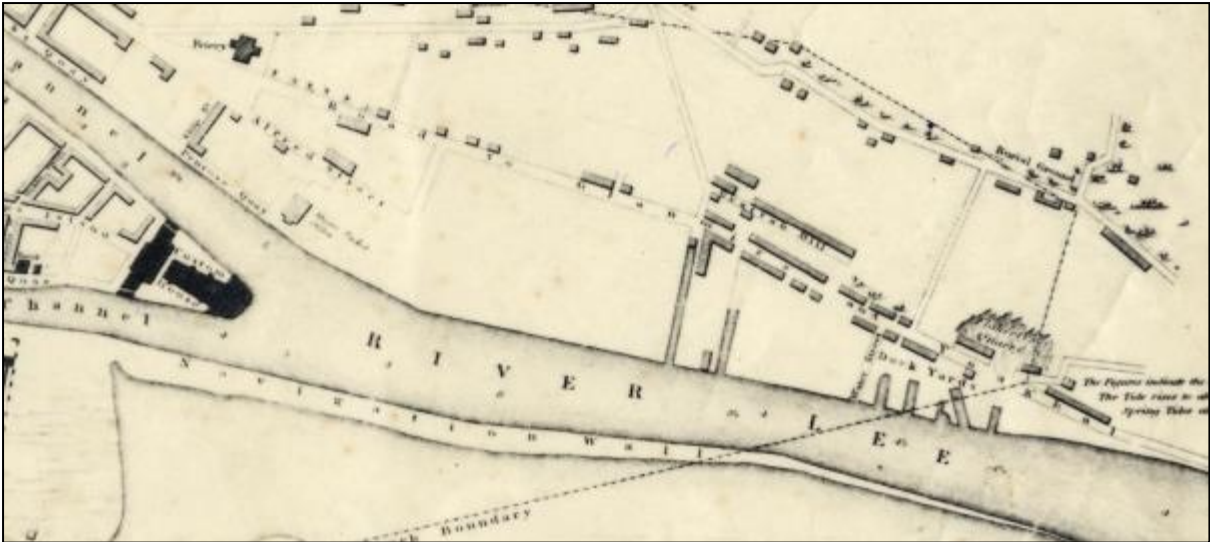


Figure 3: Holt's map of 1832 depicting west end of study area showing extent of reclamation and slipways within the area on right of image in area now occupied by McMahon Builder Provisions

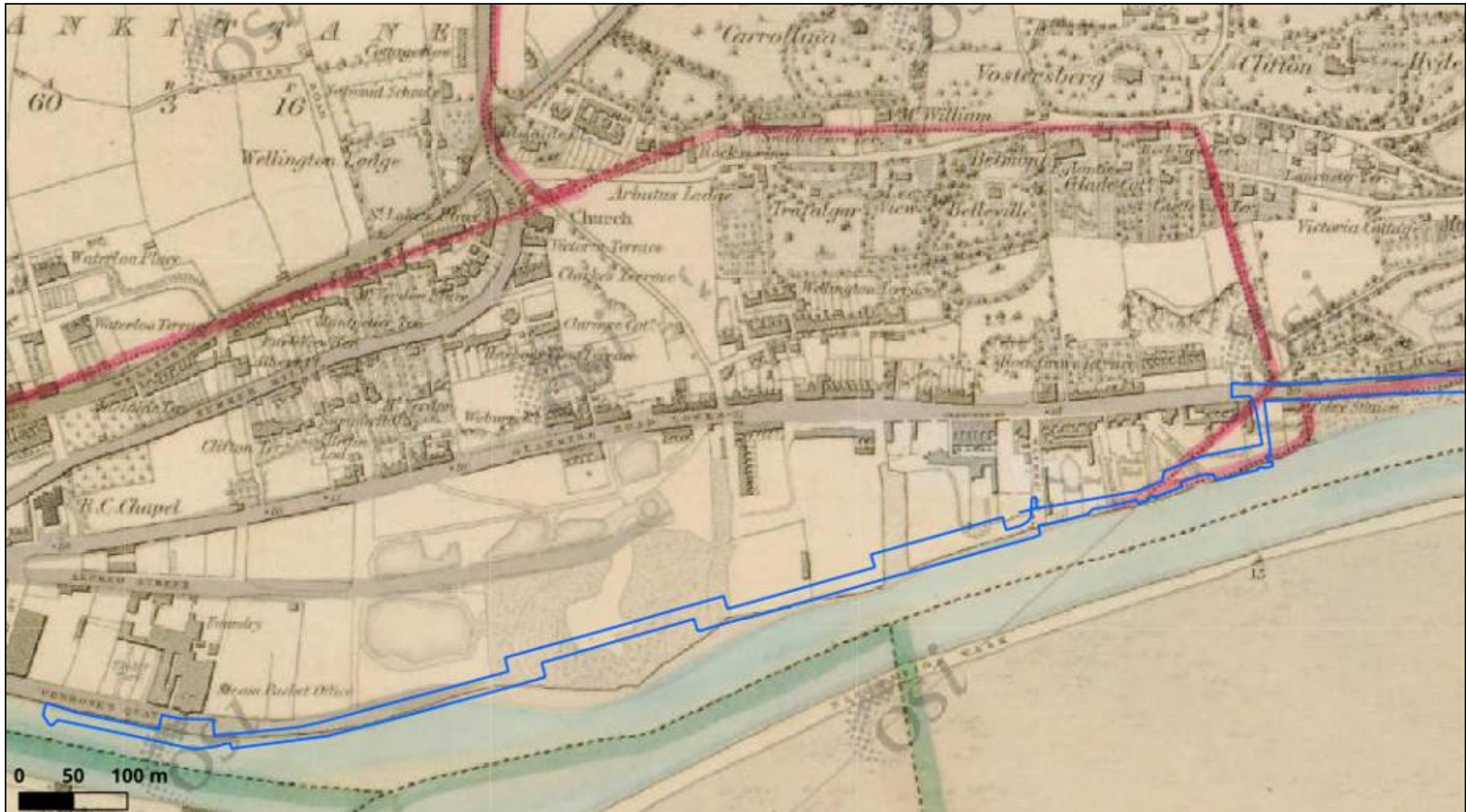


Figure 4a: Extract from 1st edition 6-inch OS map (1845) showing scheme boundary (blue line) in Penrose/Horgan Quay area (OSI Licence ref. 0003323)

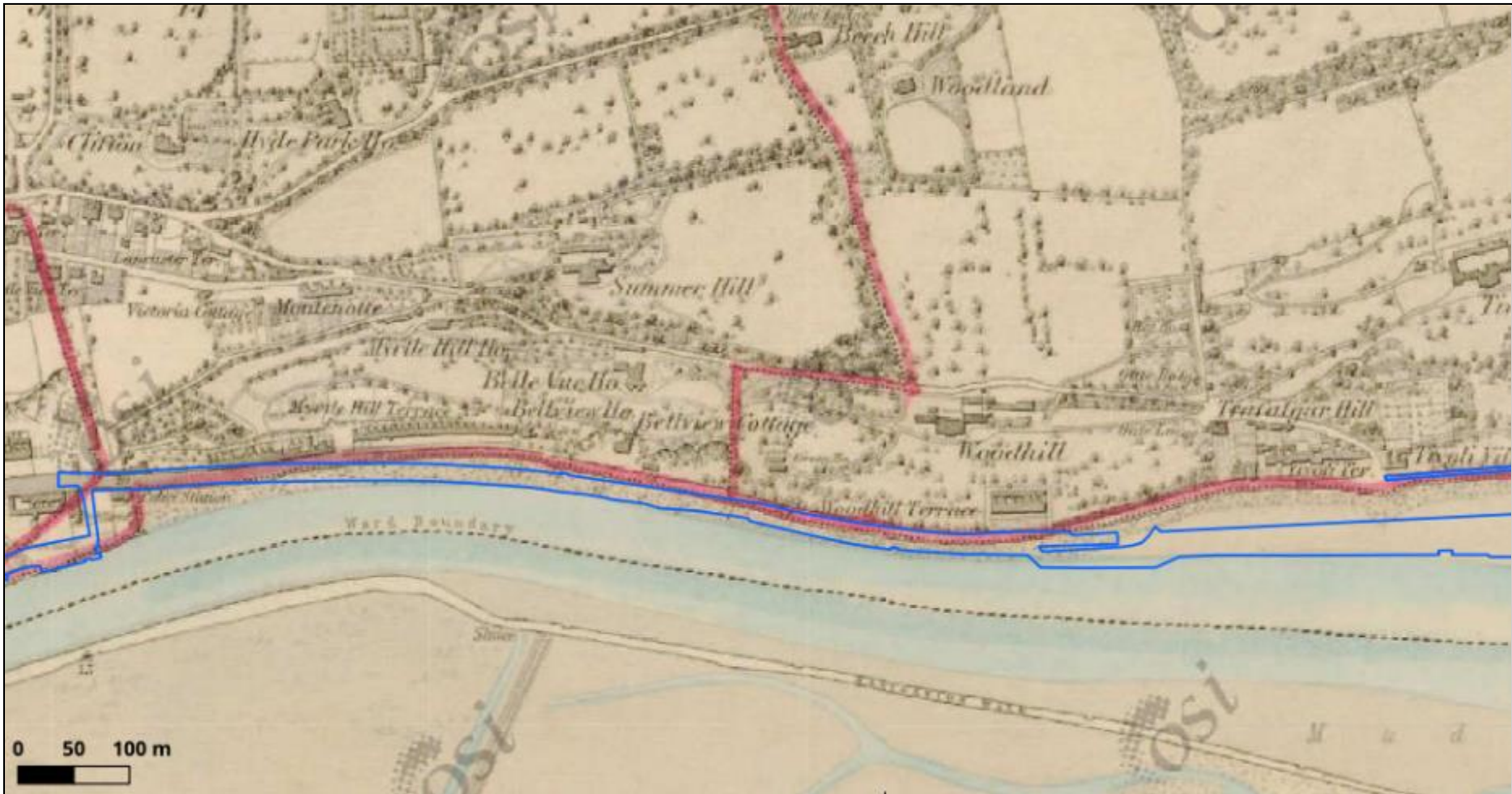


Figure 4b: Extract from 1st edition 6-inch OS map (1845) showing scheme boundary (blue line) along west end of Lower Glanmire Road (OSi Licence ref. 0003323)

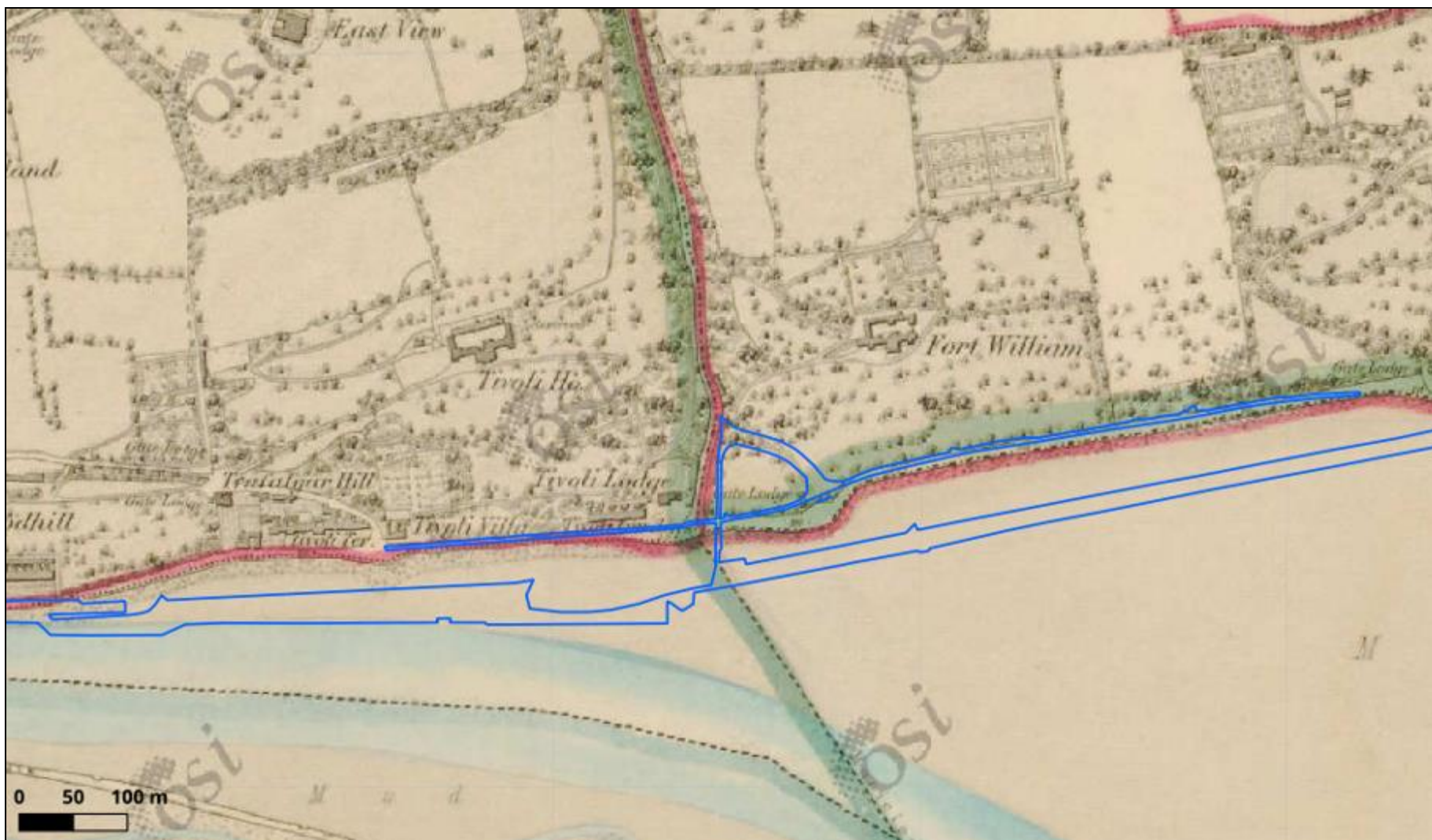


Figure 4c: Extract from 1st edition 6-inch OS map (1845) showing scheme boundary (blue line) in central area of Lower Glanmire Road (OSI Licence ref. 0003323)



Figure 4d: Extract from 1st edition 6-inch OS map (1845) showing scheme boundary (blue line) in east end of Lower Glanmire Road (OSi Licence ref. 0003323)

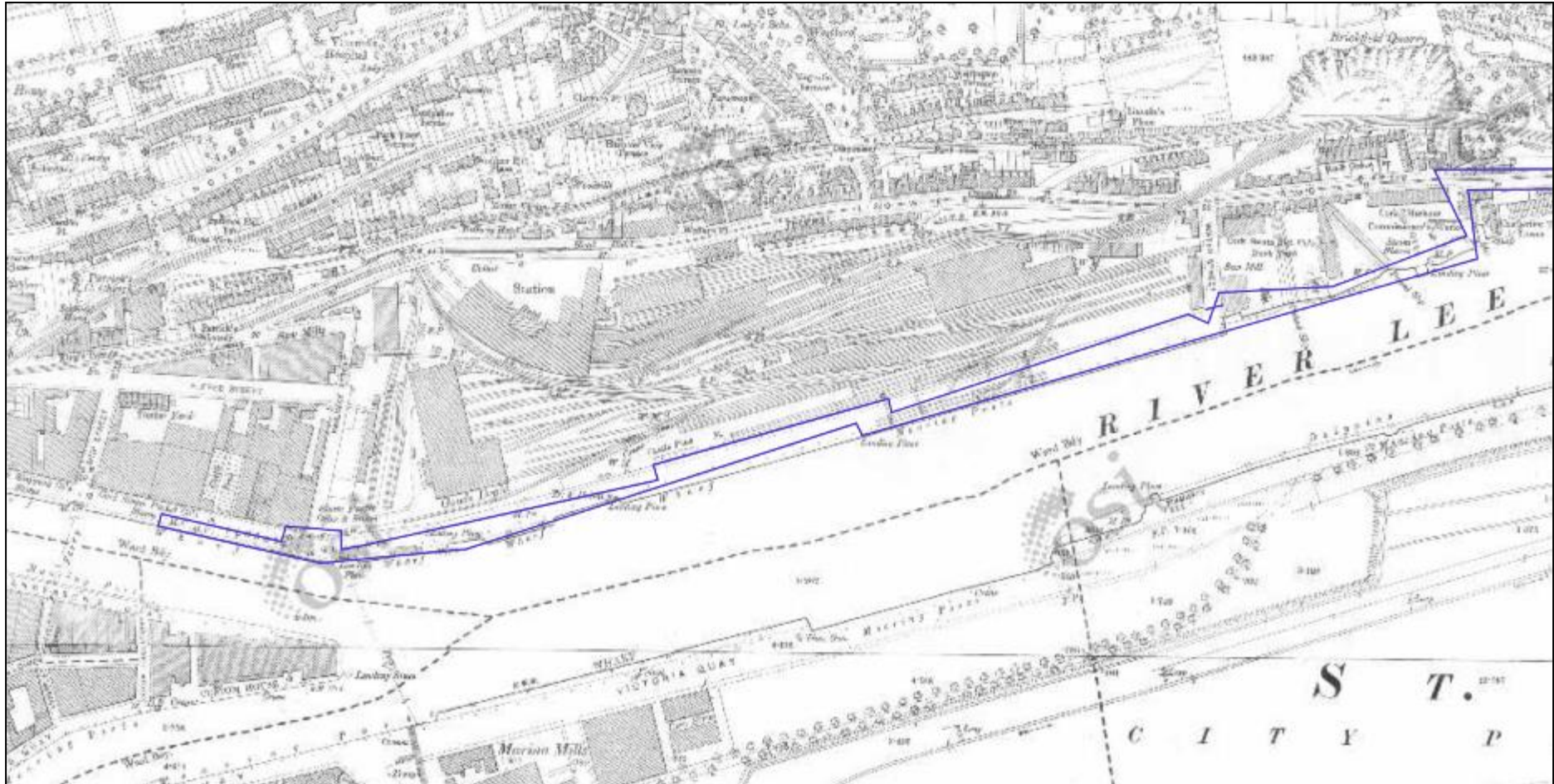


Figure 5a: Extract from 25-inch OS map (1902) showing scheme boundary (blue line) in Penrose/Horgan Quay area (OSI Licence ref. 0003323)

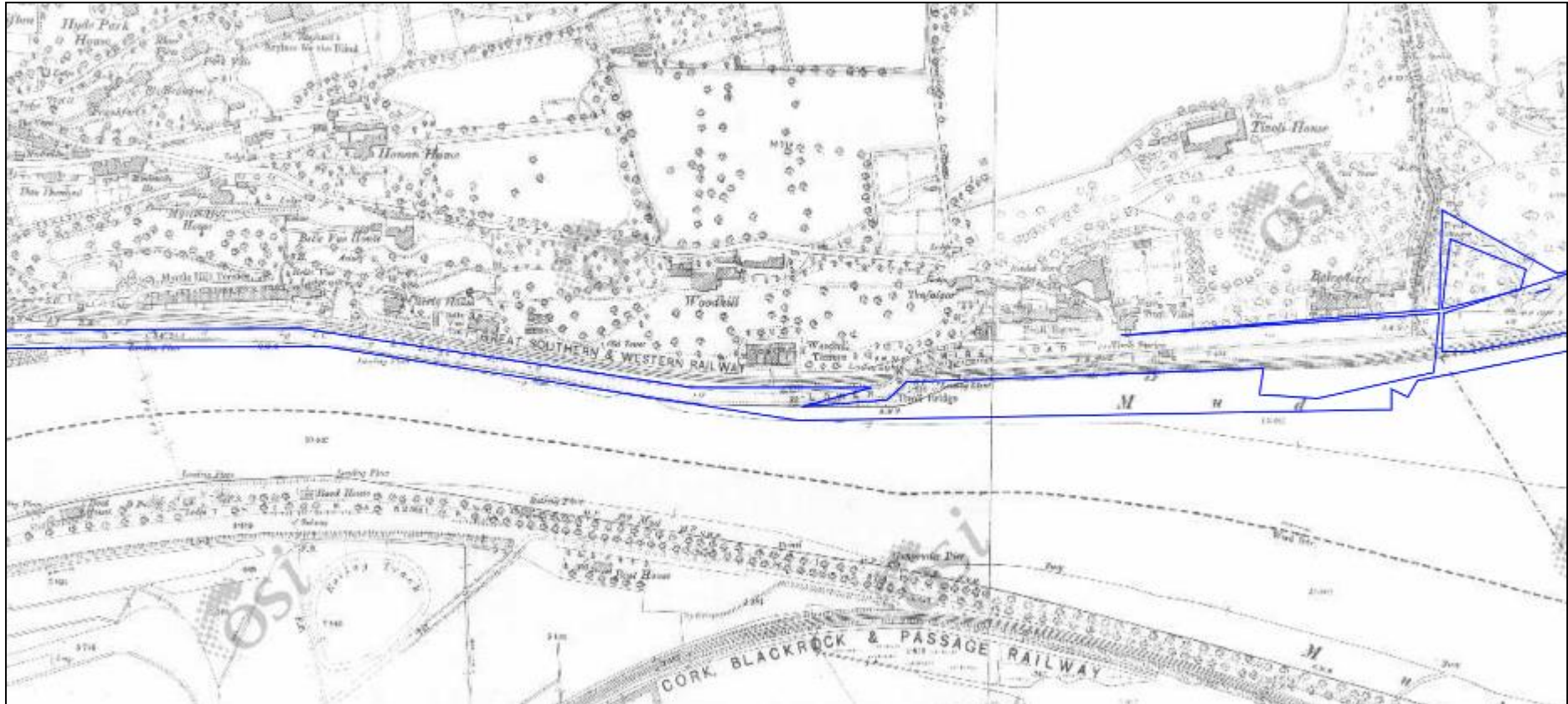


Figure 5b: Extract from 25-inch OS map (1902) showing scheme boundary (blue line) in west end of Lower Glanmire Road (OSI Licence ref. 0003323)

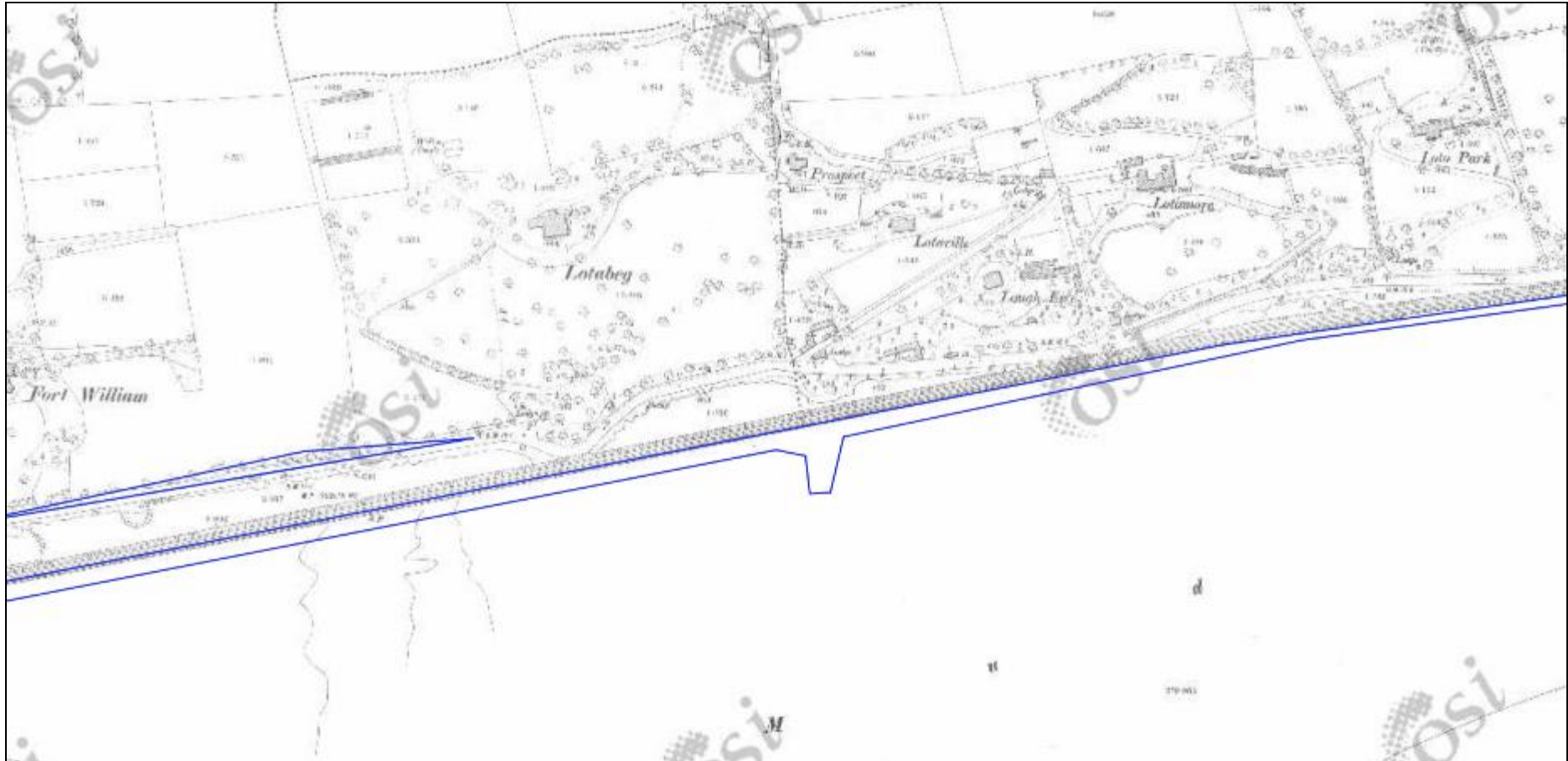


Figure 5c: Extract from 25-inch OS map (1902) showing scheme boundary (blue line) in central area of Lower Glanmire Road (OSi Licence ref. 0003323)

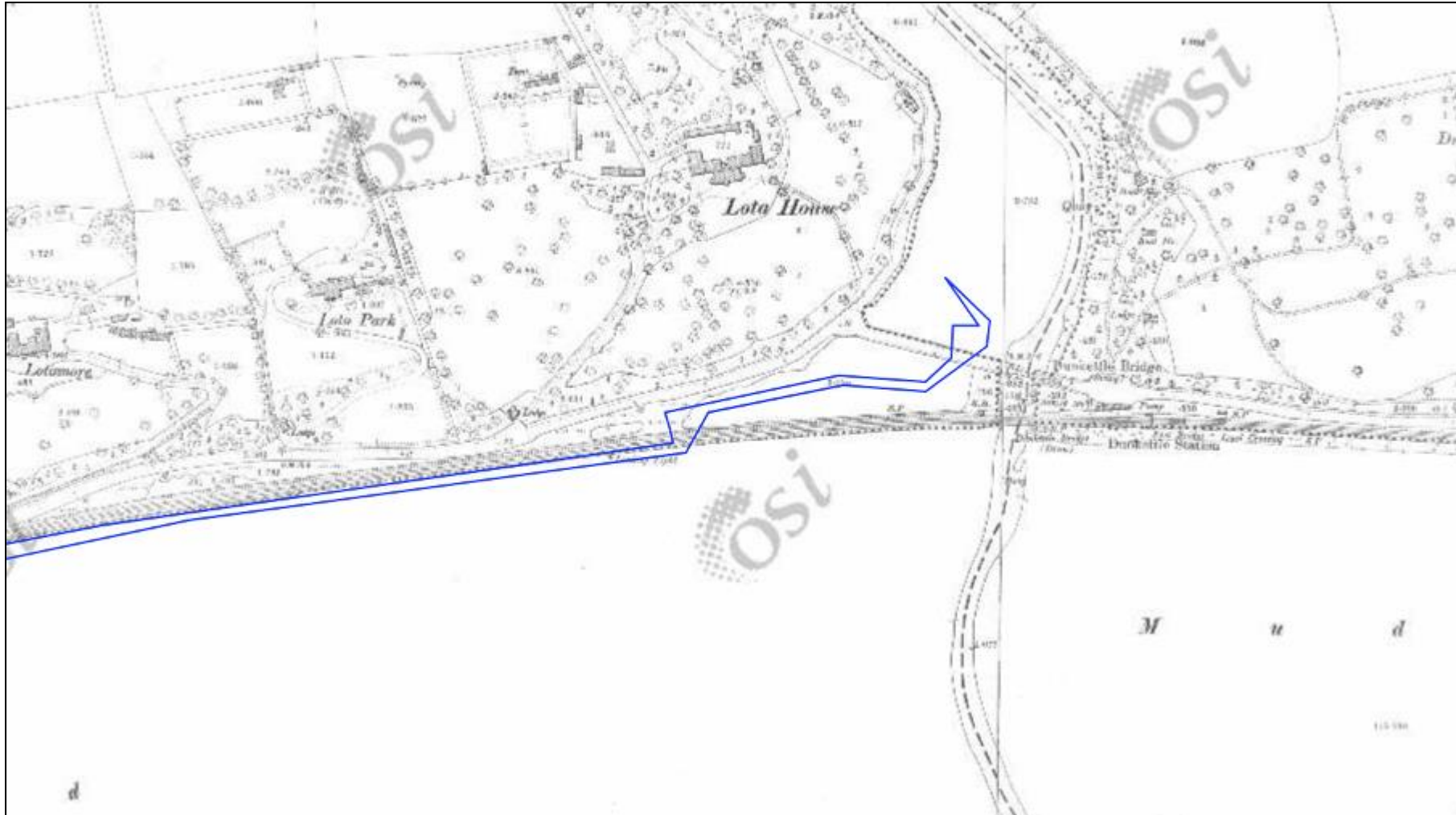


Figure 5d: Extract from 25-inch OS map (1902) showing scheme boundary (blue line) in east end of Lower Glanmire Road (OSi Licence ref. 0003323)

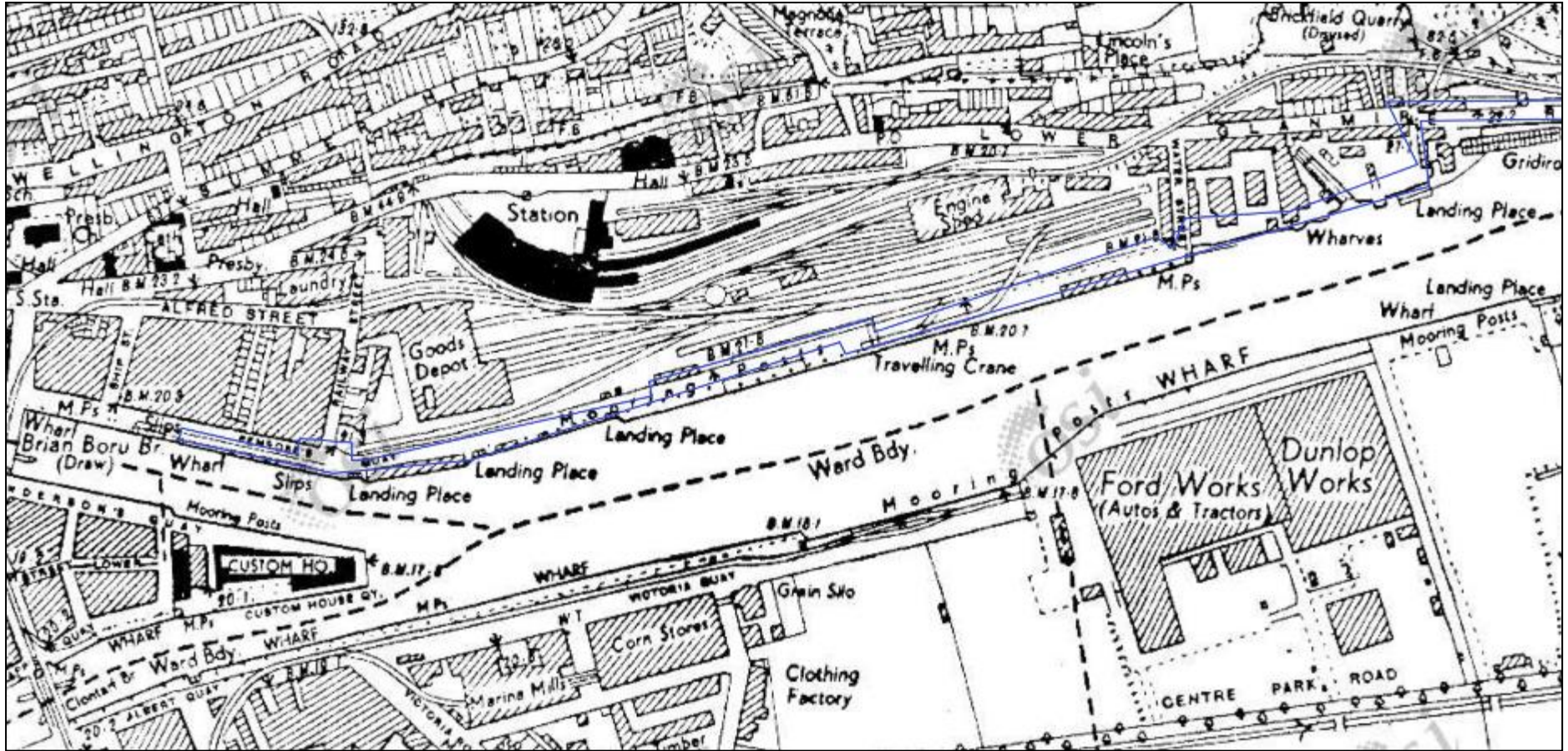


Figure 6a: Extract from 2nd edition 6-inch OS map (1956) showing scheme boundary (blue line) in Penrose/Horgan Quay area (OSI Licence ref. 0003323)

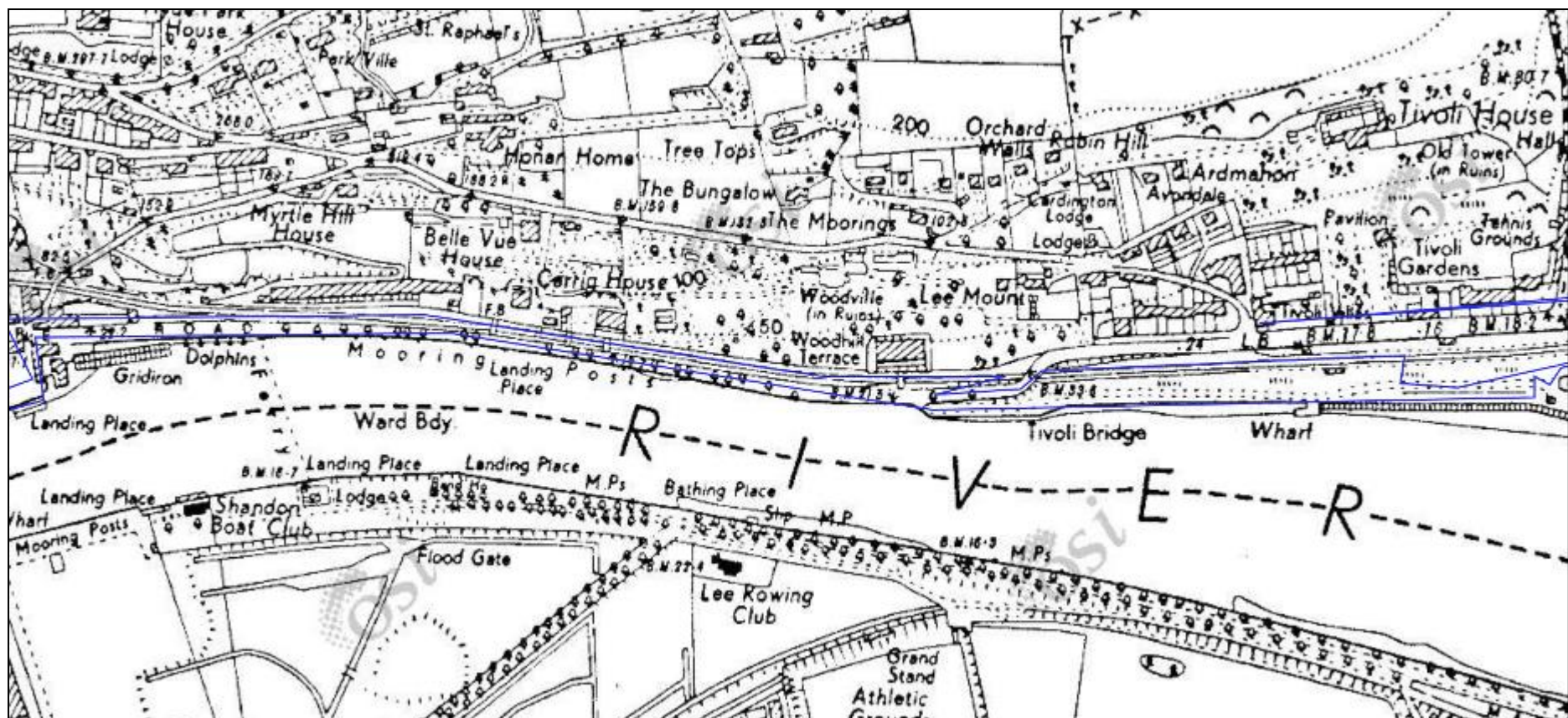


Figure 6b: Extract from 2nd edition 6-inch OS map (1956) showing scheme boundary (blue line) in west end of Lower Glanmire Road (OSI Licence ref. 0003323)

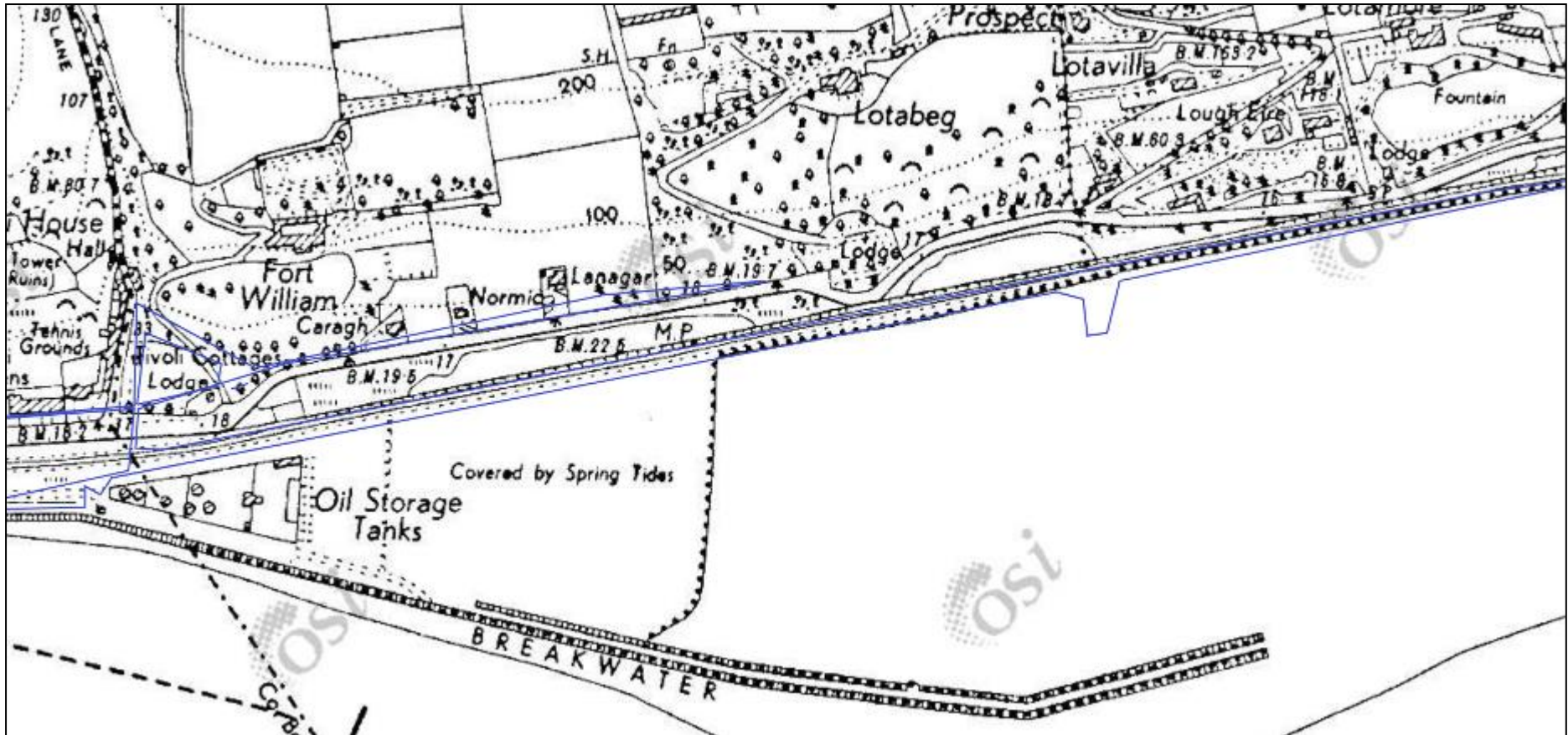


Figure 6c: Extract from 2nd edition 6-inch OS map (1956) showing scheme boundary (blue line) in central area of Lower Glanmire Road (OSI Licence ref. 0003323)

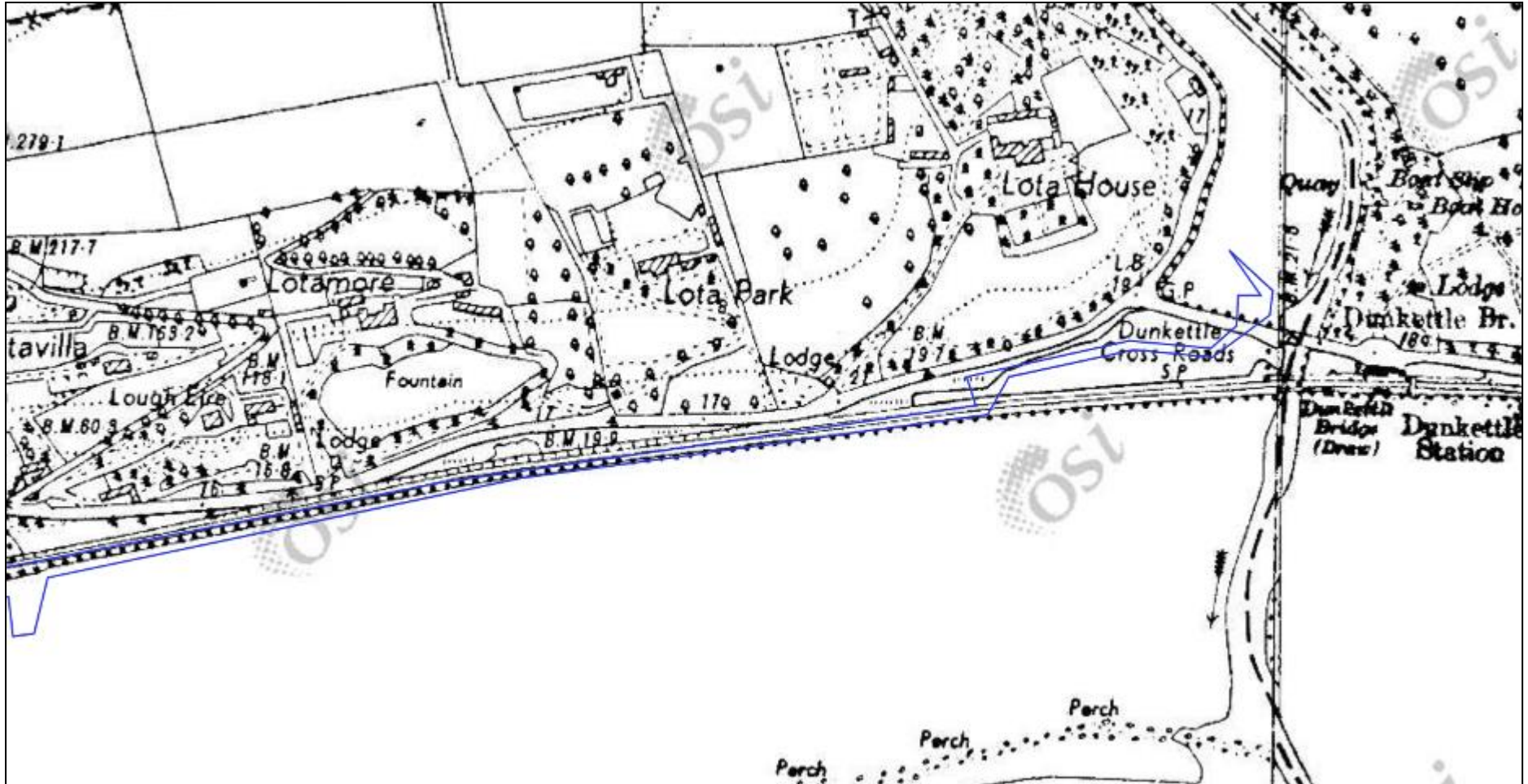


Figure 6d: Extract from 2nd edition 6-inch OS map (1956) showing scheme boundary (blue line) in east end of Lower Glanmire Road (OSI Licence ref. 0003323)

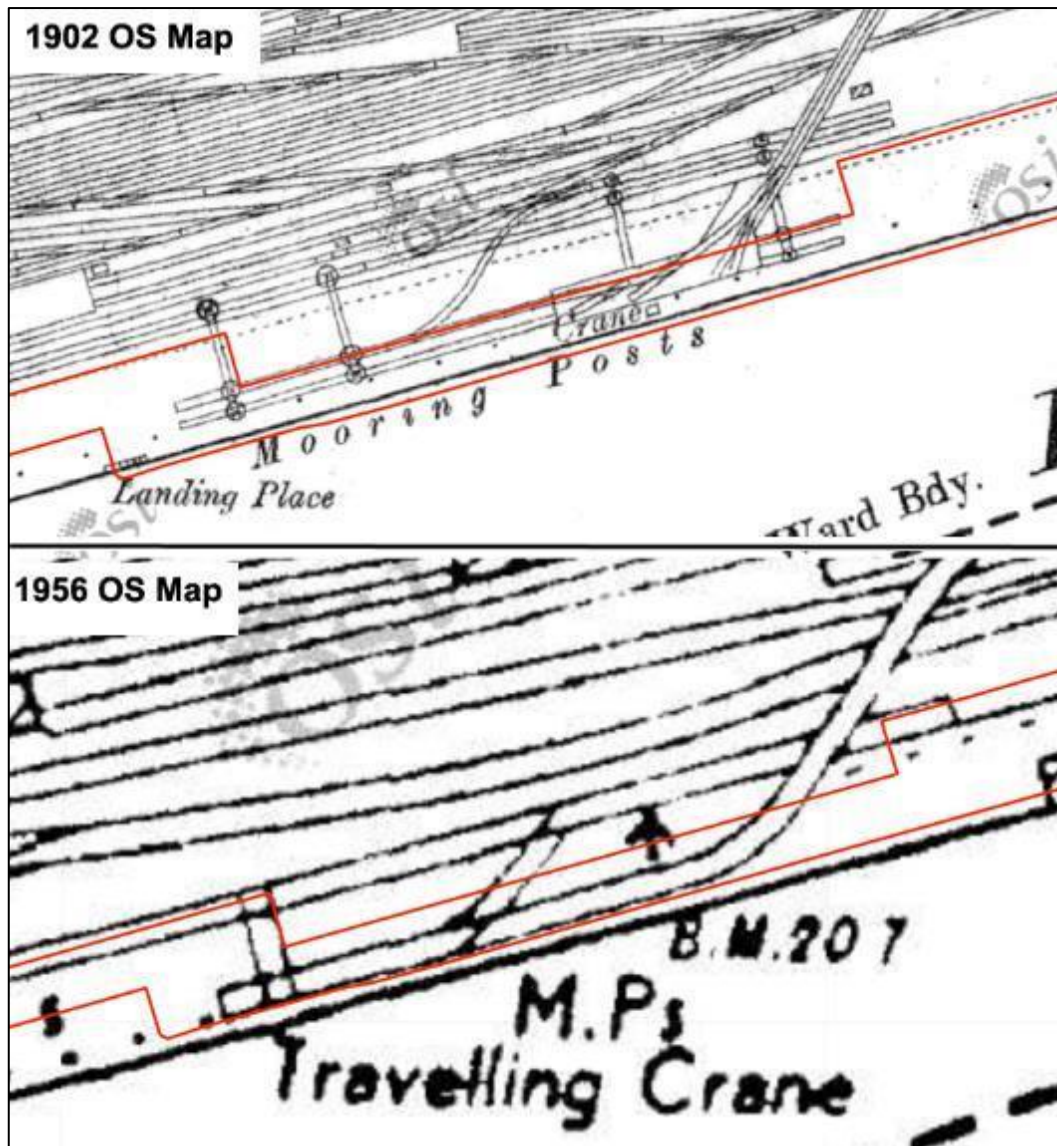


Figure 7: Combined extracts from 1902 and 1956 OS maps showing locations of rails/cranes on Horgan's Quay (scheme boundary indicated in red)

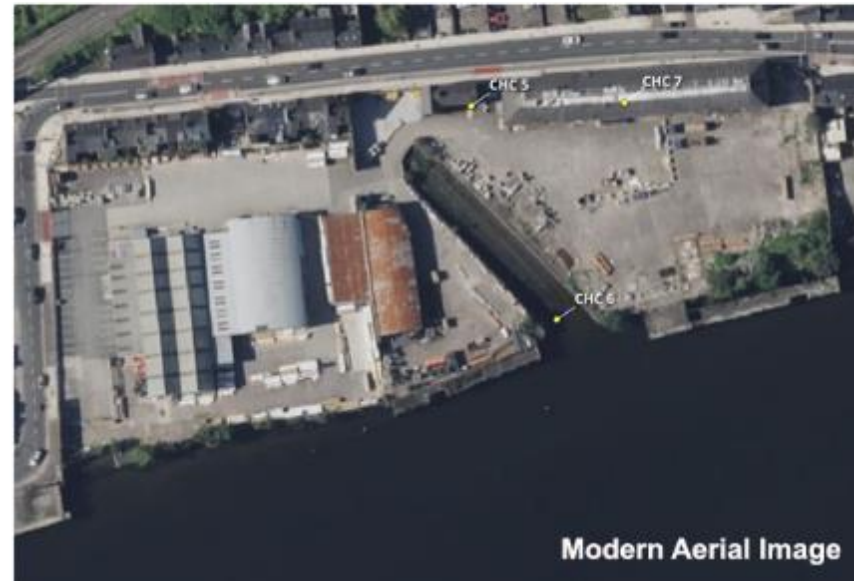
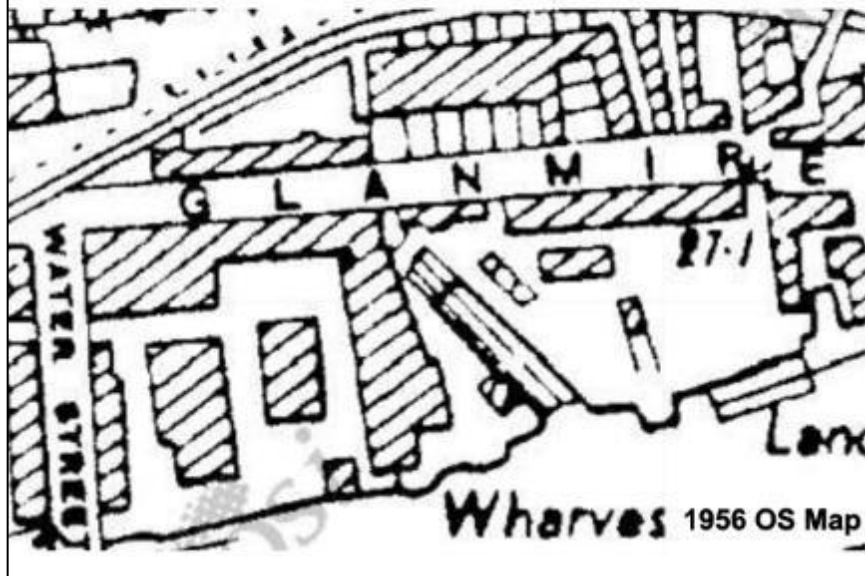
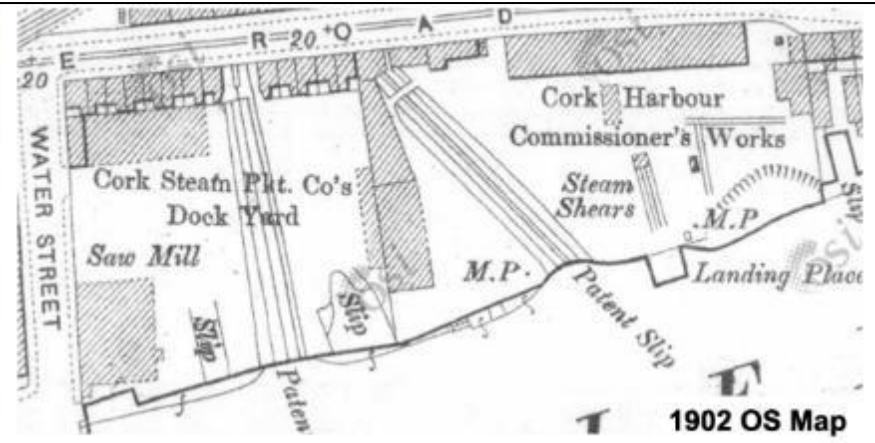
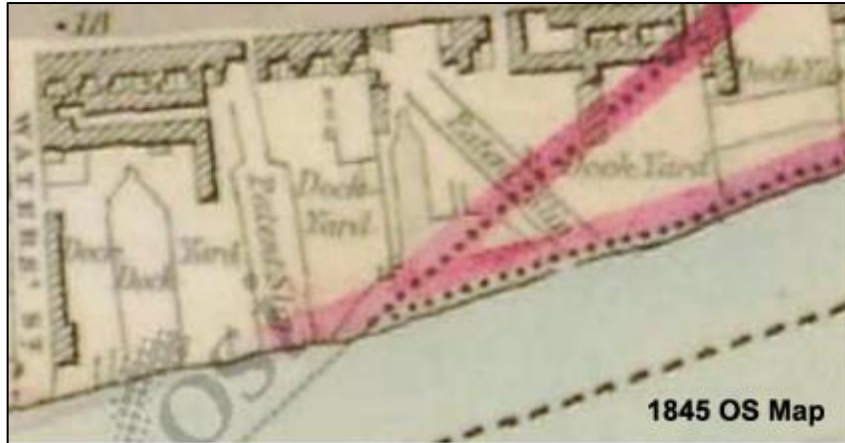


Figure 8: Extracts from 1845, 1902 and 1956 OS maps & modern aerial image of historic shipbuilding features and existing layout in McMahon property

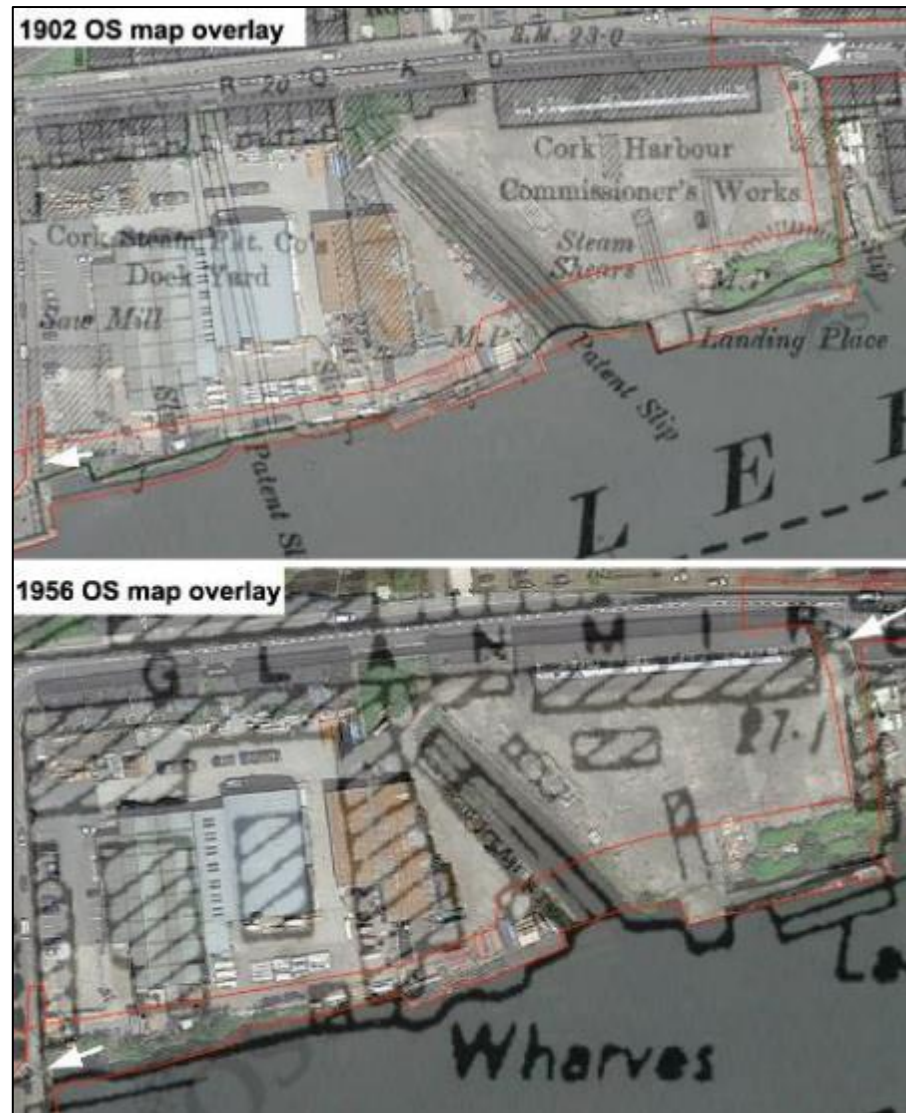


Figure 9: Extracts from 1902 and 1956 OS maps overlain on modern aerial image of McMahon's property (scheme boundary indicated in red and locations of sections of boundary walls to be removed indicated with white arrows)



Figure 10a: Extracts from Google Street View images showing extent of alterations to McMahon's west boundary wall from southwest



Figure 10b: Extracts from Google Street View images showing extent of modern removal of McMahon’s west boundary wall from northwest (section to be removed as part of proposed scheme on right)



Figure 11a: Location of Cultural Heritage Constraints within study area in Penrose/Horgan Quay area (scheme boundary shown in cyan)

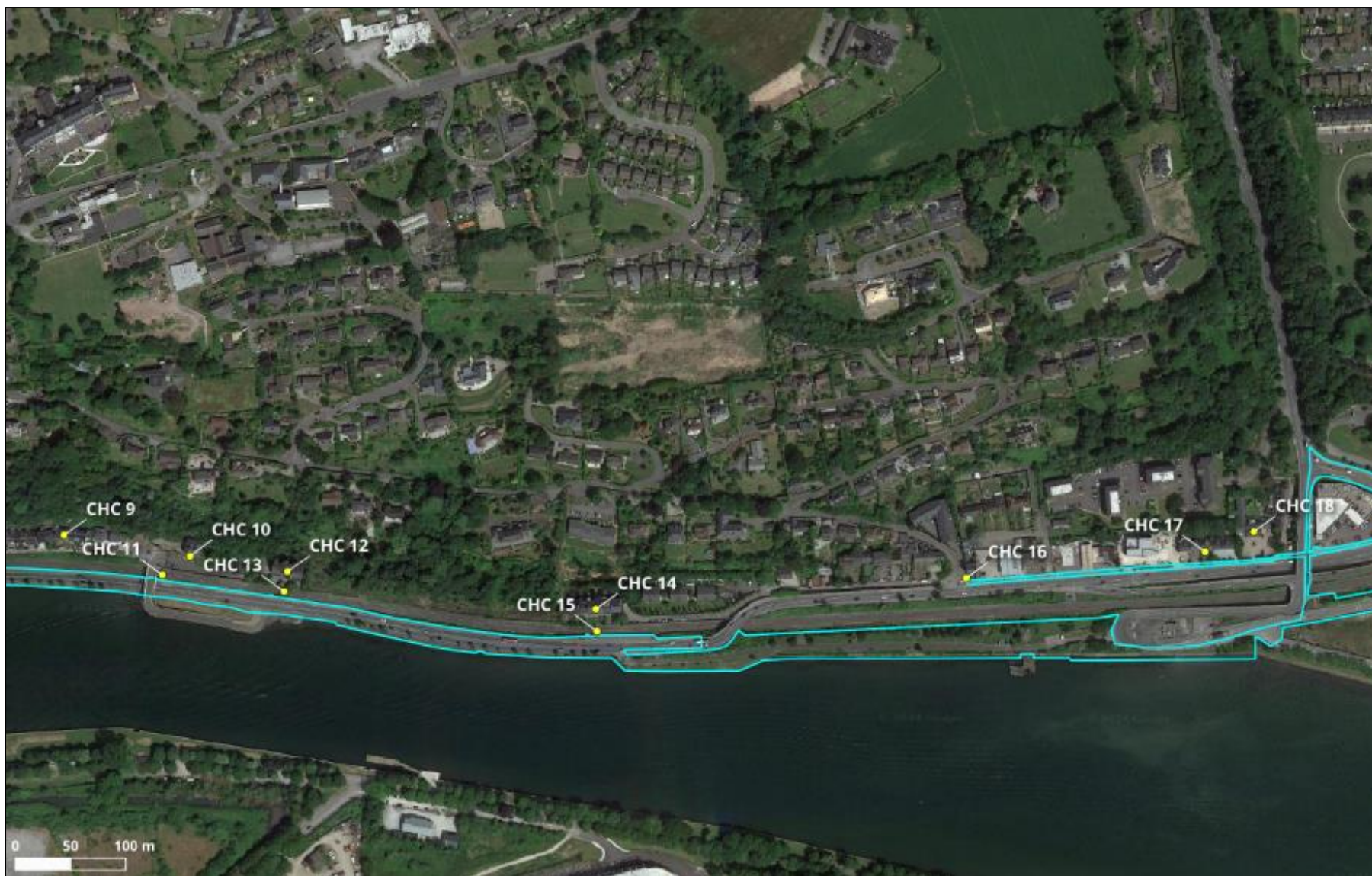


Figure 11b: Location of Cultural Heritage Constraints within study area in west end of Lower Glanmire Road (scheme boundary shown in cyan)



Figure 11c: Location of Cultural Heritage Constraints within study area in east end of Lower Glanmire Road (scheme boundary shown in cyan)

Appendix 2 Photographs



Plate 1: Existing streetscape along Penrose Quay from west



Plate 2: View of Steam Packet Office (CHC 1) from east



Plate 3: View of railway warehouse (CHC 2) from southwest



Plate 4: View of east end of Horgan's Quay existing streetscape from west



Plate 5: View of Horgan's Quay wharf (CHC 3) from east with crane rails visible in foreground



Plate 6: View of Horgan's Quay rails at intersection point within modern road from west



Plate 7: View of quay wall between wharves on Horgan's Quay



Plate 8: View of structure on west end of Horgan's Quay from east (to be retained)



Plate 9: View from south of McMahon Builder Provision extant boundary wall on Water Street showing brick detail at south terminal and adjacent metal quay post to south.



Plate 10: View from north of McMahon Builder Provision extant boundary wall on Water Street showing modern brick inset at north terminal



Plate 11: Detailed view of random rubble boundary wall showing crack in central area



Plate 12: General view from north of McMahon Builder Provision extant boundary wall on Water Street showing modern fence set on top of basal remains of reduced boundary wall with section of south end of wall to be removed indicated by arrow (cross-refer with Figures 10 a and 10b)



Plate 13: View of extant section of McMahon Builder Provision boundary wall on Water Street from east showing traces of removed concrete attached structure



Plate 14: General view of former shipyard area within south end of McMahon Builder Provision premises from west with overgrowth and stored materials visible



Plate 15: General view of former shipyard area within east end of McMahon Builder Provision premises from northwest



Plate 16: View of remaining slipway (CHC 6) within yard from northwest with concrete repairs to upper section of sidewalls and basal slideways visible



Plate 17: View of east facing side of south end of slipway (CHC 6) from east showing visible traces of random rubble construction and concrete repairs to upper section



Plate 18: View of west facing side of south end of slipway (CHC 6) from west with section of wharf extending to east visible on right side



Plate 19: View collapsing section of wharf located to east of slipway (CHC 6) which is outside scheme boundary



Plate 20: View from southeast of former Harbour Master House (CHC 5) in north end of McMahon's yard (located c.50m outside scheme boundary)



Plate 21: View of Port of Cork workshop (CHC 7) in north end of McMahon's yard from southwest (located outside scheme boundary)



Plate 22: General view of northern boundary of the McMahon yard located on east side of workshop (CHC 7) visible on left side of frame



Plate 23: View of McMahon's yard northern boundary wall from south



Plate 24: View from north of section of northern boundary wall of McMahon yard to be removed with workshop (CHC 7) visible on right side of frame



Plate 25: Detailed view from east of section of northern boundary wall of McMahon yard to be removed

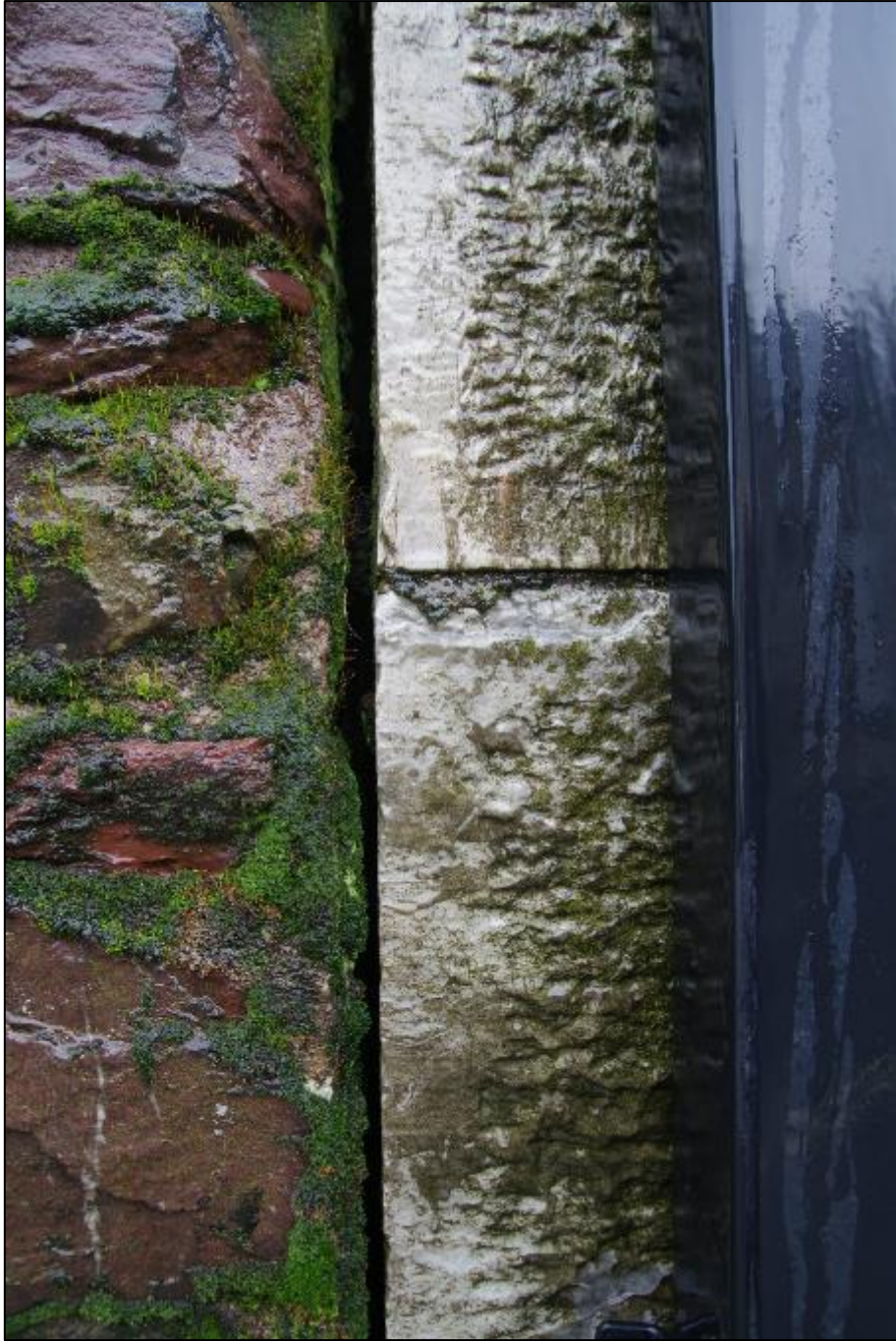


Plate 26: Detailed view of section where boundary wall adjoins the corner of the workshop building (CHC 7)



Plate 27: View of steps (CHC 8) located outside scheme boundary from southeast



Plate 28: View of existing road within Tivoli Docks area from west



Plate 29: View of Lotabeg gateway (CHC 19) from west



Plate 30: View from east towards Lotamore gate lodge (CHC 20)



Plate 31: View of gateway (CHC 21) to Lota Park property from west



Plate 32: View of Lower Glanmire Road roundabout at east end of scheme

Appendix 3 NIAH Descriptions

CHC	NIAH	Class	NIAH Description
CHC 1	20506339	Steam Packet Office	<p>Seven-bay two-storey limestone ashlar commercial building, c. 1833, with five-bay breakfront having additional three-bay breakfront with sculpture to pediment of Ionic portico; bay-window and porch additions, c. 1880. Flat and double pitched roof with Trocal sheeting and cast iron rooflights. Many late nineteenth century interior elements have been retained, including marble columns with carved and painted foliated capitals, large timber dog-leg staircase with iron balusters, mahogany glazed screens and former cash hatches. Windows have been replaced.</p> <p>An imposing large classical building with ashlar limestone facing and cut stone details to portico retaining significant elements to the interior and decorative cast iron railings to the front. Occupying a prominent site on the quays.</p>
CHC 2	20506289	Warehouse	<p>Nine-bay single-storey railway goods store, c. 1855, having squared limestone façade with cut limestone details; red brick to some of the arches; extended, c. 1950. Part of the Kent Station complex of buildings, this is an industrial building constructed of quality materials and survives in good condition. It is also prominently located as it can be seen from the quays.</p>
CHC 3	20506358	Wharf	<p>Limestone quay running along north bank of river Lee, c. 1860, having set of limestone steps. With later concrete wharf extension. Although extended with concrete, the original limestone quay is retained.</p>
CHC 4	20507180	Locomotive shed	<p>Detached railway engine shed, c. 1880, consisting of single and double height spaces, possessing cut-stone entrances and various small single storey buildings around yard. Pitched roof with apex glazing. Saw tooth roof with corrugated asbestos sheeting. Buff coloured brick with arched grid construction; articulated by blind open arcading. Round headed windows to recessed arches. Double height openings to eastern gable. Within railway yard. Limestone and sandstone perimeter all with red brick dressing.</p> <p>Well maintained and fully functioning late-nineteenth century detached railway engine shed, retaining much original external fabric. Of significance as part of the local railway complex and also as part of the railway history of Ireland.</p>
CHC 5	n/a	Former Harbour Master's House	n/a
CHC 6	n/a	Slipway	n/a
CHC 7	20507163	Former Port of Cork workshops	<p>Detached twelve-bay single-storey double-height random sandstone workshop, c. 1890. Hipped roof with apex glazing. Random sandstone walls with dressed limestone quoins and dressings. Camber headed window openings with metal windows and large arched openings at each end with timber double doors. Street and river frontage, and large enclosed yard with additional workshops and stores. Well maintained and very intact detached late-nineteenth century workshop, retaining much original external fabric. An</p>

CHC	NIAH	Class	NIAH Description
			important site with potential vulnerability to development and having relevance to the history of the Harbour Commissioner's and Cork's docklands.
CHC 8	20507128	Steps	<p>Flight of pavement steps with limestone risers, c. 1860. Including some stone steps, and a boundary wall to either side. Modern handrail inserted.</p> <p>Nineteenth-century steps with limestone risers, including some stone steps, having importance as surviving example in reasonable condition, where other similar examples have been lost. These steps form part of an interesting group with similar nineteenth-century paving schemes. Steps such as these are found in many location throughout the city, and contribute to the unique character of the city. These paving schemes utilised cut limestone, which in many cases has been replaced or covered with concrete.</p>
CHC 9	20863180 & 20863148 to 20863153	Myrtle Hill Terrace	<p><i>NIAH 20863180</i> Terraced three-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks, cast-iron rooflights and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and timber casement windows. Round-headed door opening in moulded render surround with engaged pilasters. Ionic columns flanking doorway with spider web fanlight over fluted frieze, six-panelled timber door and limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. The terrace is located on a height overlooking the Cork-Youghal railway line.</p> <p>Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces further to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features.</p> <p><i>NIAH 20863148</i> Terraced three-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks, cast-iron rainwater goods and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and uPVC windows. Round-headed door opening in moulded render surround with engaged pilasters. Ionic columns flanking doorway with spider web fanlight over fluted frieze, replacement six-panelled timber door and limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. Bounded by rendered wall with square-profiled gate piers holding wrought-iron pedestrian gate with spear finials. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features.</p>

CHC	NIAH	Class	NIAH Description
			<p data-bbox="880 268 1055 292"><i>NIAH 20863149</i></p> <p data-bbox="880 304 2007 655">Terraced three-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks, cast-iron rainwater goods and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sill and uPVC windows. Round-headed door opening in moulded render surround with engaged pilasters. Ionic columns flanking doorway with spider web fanlight over fluted frieze, six-panelled timber door and limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. Bounded by rendered wall with wrought-iron pedestrian gate having spear finials. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features.</p> <p data-bbox="880 695 1055 719"><i>NIAH 20863150</i></p> <p data-bbox="880 732 2007 1147">Terraced two-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and six-over-six timber sliding sash windows. Round-headed door opening in slightly projecting moulded render surround having Doric columns supporting spider web fanlight with moulded timber archivolt, panelled soffit and reveals, seven-panelled timber door with recent glazed panels inserted, and limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. Bounded by rendered wall with square-profiled gate piers holding wrought-iron pedestrian gate with spear finials. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features, including the fine doorcase and timber sliding sash windows.</p> <p data-bbox="880 1187 1055 1211"><i>NIAH 20863151</i></p> <p data-bbox="880 1224 2007 1375">Terraced two-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and six-over-six timber sliding sash windows. Round-headed door opening in slightly projecting moulded render surround having Doric columns supporting spider web fanlight with moulded timber archivolt, panelled soffit and reveals, seven-panelled timber door with</p>

CHC	NIAH	Class	NIAH Description
			<p>limestone threshold. House set on a height and approached by a flight of limestone steps from a private road. Bounded by rendered wall with square-profiled gate piers holding wrought-iron pedestrian gate with spear finials. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features, including the fine doorcase and timber sliding sash windows.</p> <p><i>NIAH 20863152</i> Terraced two-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and one-over-one timber sliding sash windows. Round-headed door opening in slightly projecting moulded render surround with Doric columns supporting spider web fanlight with moulded timber archivolt, panelled soffit and reveals, seven-panelled timber door with two glazed panels recently inserted, and limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. Bounded by render wall surmounted by wrought-iron railings with cast-iron gate piers holding wrought-iron pedestrian gate with spear finials, original wrought-iron railings at road level. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features, including the fine doorcase and timber sliding sash windows.</p> <p><i>NIAH 20863153</i> Terraced two-bay three-storey house, built c.1830. Pitched slate roof with rendered chimneystacks and cast-iron rainwater goods. Smooth rendered walls with projecting rendered eaves course. Square-headed window openings with stone sills and one-over-one timber sliding sash windows. Round-headed door opening in slightly projecting moulded render surround having Doric columns supporting spider web fanlight with moulded timber archivolt, panelled soffit and reveals, six-panelled timber door with limestone threshold. House set at a height and approached by a flight of limestone steps from a private road. Bounded by render wall surmounted by replacement wrought-iron railings, original wrought-iron railings having acorn finials at road level, wrought-iron pedestrian gate at cast-iron gate piers. The terrace is located on a height overlooking the Cork-Youghal railway line. Set on a height overlooking the River</p>

CHC	NIAH	Class	NIAH Description
			Lee, access to these houses was radically altered when the Cork to Youghal railway line opened, not long after the terrace was constructed. It forms part of a terrace of large houses, which together with the terraces to the east, are unusually grand in scale for the Glanmire Road. This building retains its original proportions and many noteworthy features, including the fine doorcase and timber sliding sash windows.
CHC 10	20863157	Carrig House	Detached five-bay two-storey house, built c.1820, with single-storey crenellated single-storey former entrance bay to west and single-storey screen to east. Currently not in use. Hipped slate roof with coupled rendered corbelled chimneystacks and cast-iron gutters on overhanging eaves. Smooth rendered walls. Square-headed window openings with label mouldings and limestone sills. Trefoil window heads visible to ground floor. All windows blocked up. Square-headed door opening to crenellated porch now blocked up. Set in its own grounds, accessed via recent timber double gates. Set on a height overlooking the Cork to Youghal railway line and the River Lee. Formerly known as Bellvue House and later as Carrig House, this house retains its symmetry and graceful proportions, despite its derelict condition. The high and wide roofline still dominates the Lower Glanmire Road and the building continues to make a valuable contribution to the surrounding area. The tall grouped chimneystacks, render detail and carved trefoil headed windows are among the many surviving notable features.
CHC 11	20863156	Foot bridge	Single-span cast-iron footbridge, erected c.1855, over Cork-Youghal Railway. Supported on red brick piers with round-headed arches cast-iron tie-plates. Lattice steps supported on cast-iron columns with decorative cast-iron railings balustrade to deck. Located in close proximity to recent flyover bridge. Built to provide access to private houses cut off from the road by the construction of the Cork to Youghal railway line, this mid nineteenth-century pedestrian bridge is a significant contributor to the architectural and social heritage of the city, and it makes a notable addition to the streetscape. A functional structure, it is enhanced by artistic details, such as the decorative tie-plates and pier details. The bridge is an interesting example of the materials and design utilised by railway companies in the nineteenth century.
CHC 12	20863158	Bellevue Villas Housing Terrace	Terrace of three three-bay three-storey over basement houses, built c.1850. Some now converted to apartments. Pitched slate roofs with rendered corbelled chimneystacks and cast-iron rainwater goods. Ruled-and-lined rendered walls. Square-headed window openings with limestone sills with six-over-six (second floor) and nine-over-six (ground and first floors) timber sliding sash windows. Round-headed door openings with slightly projecting timber surrounds, timber spoked fanlights and fluted Doric columns flanking panelled timber doors accessed via flight of five limestone steps with wrought-iron railings. Basement area bounded by wrought-iron railings with alternating spear and fleur-de-lys finials. Access across Cork to Youghal railway line via cast-iron pedestrian bridge. A well-maintained terrace of well-proportioned mid nineteenth century houses. In common with contemporary houses in the area, the terrace is set on an elevated site overlooking the River Lee and makes the most of its south facing site, maximising light and views. The retention of the original fenestration and handsome timber doorcases contributes to its character.

CHC	NIAH	Class	NIAH Description
CHC 13	20863156	Foot bridge	<p>Single-span cast-iron footbridge, erected c.1855, over Cork-Youghal Railway. Supported on red brick piers with round-headed arches cast-iron tie-plates. Lattice steps supported on cast-iron columns with decorative cast-iron railings balustrade to deck.</p> <p>Built to provide access to private houses cut off from the road by the construction of the Cork to Youghal railway line, this mid nineteenth-century pedestrian bridge is a significant contributor to the architectural and social heritage of the city, and it makes a notable addition to the streetscape. A functional structure, it is enhanced by artistic details, such as the decorative tie-plates and pier details. The bridge is an interesting example of the materials and design utilised by railway companies in the nineteenth century.</p>
CHC 14	20863163	Woodhill Villas Housing Terrace	<p>Terrace of six three-bay three-storey over basement houses, built c.1830. Some now converted to apartments. Pitched slate roofs with rendered chimneystacks and cast-iron rainwater goods. Some houses with recent flat-roofed dormer windows to front (south) elevation. Square-headed window openings with limestone sills and two-over-two timber sliding sash windows. Round-headed door openings in slightly projecting timber surrounds with fluted timber Doric columns flanking three raised and fielded timber panelled doors, timber spoked fanlights and accessed via flight of limestone steps. Basement area bounded by wrought-iron railings with alternating spear and fleur-de-lys finials. Garden to fore bounded by wrought-iron railings with alternating spear and fleur-de-lys finials. Terrace accessed by cast-iron pedestrian bridge over railway line to south.</p> <p>Woodhill Terrace took its name from Woodhill House, a significant earlier house built in the eighteenth century and the home of the Cooper Penrose family. The terrace has retained its simple early symmetrical façade while the survival of the original fenestration and attractive timber doorcases contributes to its character. Further interest is added by the access route via a highly-detailed cast-iron pedestrian bridge to the front which was built after the houses were cut off from the main road by the construction of the Cork to Youghal railway line in the mid nineteenth century.</p>
CHC 15	20863164	Foot bridge	<p>Single-span cast-iron footbridge, erected c.1855, over Cork-Youghal Railway. Supported on red brick piers with round-headed arches cast-iron tie-plates. Lattice steps supported on cast-iron columns with decorative cast-iron railings balustrade to deck.</p> <p>Built to provide access to private houses cut off from the road by the construction of the Cork to Youghal railway line, this mid nineteenth-century pedestrian bridge is a significant contributor to the architectural and social heritage of the city, and it makes a notable addition to the streetscape. A functional structure, it is enhanced by artistic details, such as the decorative tie-plates and pier details. The bridge is an interesting example of the materials and design utilised by railway companies in the nineteenth century.</p>
CHC 16	20864008	Post box	<p>Freestanding cast-iron post box, c.1890. Shallow domed capping with dentil enriched sides and hinged door bearing the V.R. insignia in raised lettering.</p>

CHC	NIAH	Class	NIAH Description
			The design of pillar post boxes was standardised in 1859 and this example survives from the latter years of Queen Victoria's reign. While some post boxes had their doors with the monarch's insignia removed in the 20th century, this example has remained intact and has stood in this location for over a century.
CHC 17	20864029	Tivoli Gardens Housing Terrace	<p>Terrace of five four-bay two-storey houses, built c.1830. with one house now a guest house and another in use as offices. Pitched slate and artificial slate roofs with rendered chimneystacks having clay pots and cast-iron rainwater goods. Some replacement rainwater goods. Dormer windows inserted to central house. Smooth rendered walls with render removed from central house. Square-headed window openings with stone sills and one-over-one timber sash windows. Camber-headed tripartite window openings to ground floor. Replacement timber six-over-six sash windows and uPVC windows to some house. Round-headed doorways with timber pedimented doorcase to western house and replacement doors throughout. Variety of rendered and exposed low rubble stone walls to the boundaries of the front gardens to south.</p> <p>This early-nineteenth century terrace was constructed at a time when expansion to the west of the city was only beginning, and the urban appearance of these houses would have contrasted sharply with the villas and country houses which occupied this area at that time. Despite the replacement of windows, the original scale and form of the south-facing front elevations of these terraced buildings have been retained, as have the roof profiles of all but one house.</p>
CHC 18	20864009	Belvedere Lodge	<p>Detached double-pile three-bay two-storey house, built c.1830, with single-storey wing to east. Remodelled c.1890 with canted bays windows and single-storey veranda added to south and two-storey extension to west. Now in use as a guesthouse, with three-bay two-storey extension to north-west and conservatories to south-east and south-west. Pitched artificial slate roofs having crenellated parapet to south, smooth rendered chimneystack and cast-iron rainwater goods. Smooth rendered walls. Square-headed window openings with pedimented hood mouldings to first floor south elevation and uPVC replacement windows. One-over-one timber sash windows to canted bays. Round-headed door opening with moulded render surround, timber-and-glazed panelled door and fanlight. Veranda to south with hipped slate roof and supported on wrought-iron trellises. Ruined three-bay two-storey semi-circular rubble stone and brick garden folly to east entrance having small quatrefoil openings. Rendered plinth walls having cut limestone coping and replacement railings to south with square-profile limestone piers having cut limestone caps to replacement gates.</p> <p>This building was formerly known as Tivoli Lodge and was associated with Tivoli House to the north-west, now demolished. The ornate canopy was added in the late-Victorian period when such ornate architectural features were in fashion. The castellated parapet and decorative garden folly are picturesque features which date from a time when this area of the city was much less developed and considered to be well outside the city boundaries.</p>

CHC	NIAH	Class	NIAH Description
CHC 19	20864012 20864013	Lotabeg gate and lodge	<p><i>NIAH 20864012</i> Detached single-bay single-storey gate lodge, built c.1800, having pedimented porch to south and lean-to extension to north. Hipped slate roof having brick chimneystack with ceramic pot. Squared coursed limestone and sandstone walls with cut limestone quoins and pediment. Square-headed window openings with limestone surrounds and replacement aluminium windows. Square-headed opening to porch, door not visible. Aluminium frames to square-headed window openings. Although modest in size this gate lodge has been constructed using the finest materials and displays skilled stone masonry in the cut limestone details. The form of this building is typical for gate lodges of this period, but the classical portico on the main elevation reflects the status of the main house. It forms an imposing entrance with the associated entrance gates.</p> <p><i>NIAH 20864013</i> Freestanding Ionic triumphal arch gateway, built c.1820, forming entrance to Lotabeg. Central vehicular opening flanked by lower pedestrian openings. Ashlar limestone walls, cut stone plinths, moulded stepped cornice and engaged Ionic columns on tooled stone plinths flanking vehicular archway and supporting projecting ends of stepped moulded cornice. Carved limestone sculpture of a lying dog tops the vehicular entrance. Single-leaf ornate wrought-iron gates to pedestrian gateways and double-leaf wrought-iron gates to vehicular entrance. Wrought iron lantern stands over each of the pedestrian gateways. Coursed rubble stone estate walls adjoin the gateway at both sides. This monumental entrance, attributed to George Richard Pain, exhibits ornate detailing and fine craftsmanship in both the stonework and wrought ironwork and forms a grand entrance to Lotabeg. Situated on the Lower Glanmire Road the gateway is a landmark structure, with the unusual sculpture of the dog which tops the main gateway especially recognisable.</p>
CHC 20	20864019	Lotamore gate lodge	<p>Detached two-bay single-storey gate lodge, built c.1815, with flat-roofed porch, c.1980, to east. Hipped slate roof with projecting eaves, centrally placed rendered chimneystack and timber eaves boards. Rendered walls with raised render plinth, fascia to eaves and intermittent pilasters. Square-headed window openings with uPVC windows. Square-headed door opening to porch with timber panelled door. Cast iron railings, c.1880, on rendered plinth to south and east having cut limestone pedestrian gate piers. Cast-iron vehicular access gate piers to the avenue to Lotamore bearing Hive Iron Works manufacturer's stamp. This modest building has a design and roof profile typical of gate lodges of this period. It marks the entrance to Lotamore which is situated further back off the road and is one of the most architecturally significant houses of this area. The decorative cast-iron railings display a high level of craftsmanship and the cast-iron gate piers are an unusual substitute for the more typical limestone piers.</p>

CHC	NIAH	Class	NIAH Description
CHC 21	20864022	Lota Park gate lodge	<p>Entrance gateway, built c.1810, as entrance to Lotabeg. Comprising freestanding square-headed pedestrian entrances flanking vehicular access. Limestone square-profile piers with raised plinths, incised panels and stepped caps supporting fascia and cornice with parapet above. Wrought-iron pedestrian gates. Located to south-west of house.</p> <p>These impressive cut stone gateways mark the entrance to the former house, located on an elevated site further along the avenue. The pedestrian gates are an early surviving example of decorative wrought-iron work and display a high level of craftsmanship which can also be seen in the railings at the main house.</p>